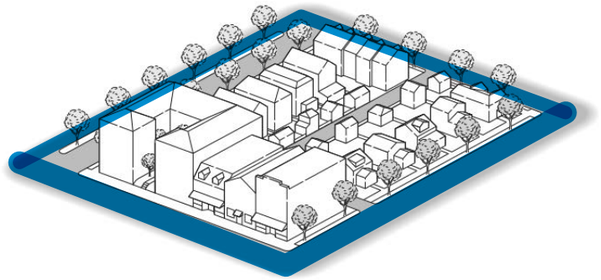
HILLCREST URBAN DESIGN WORK GROUP

Meeting 3 – Regulatory and Design Basics for Streets and Blocks

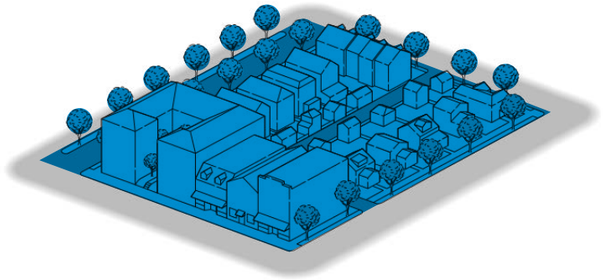
Birdseye of Hillcrest Site, St. Paul, MN – Google

DESIGN SCALE FRAMEWORK



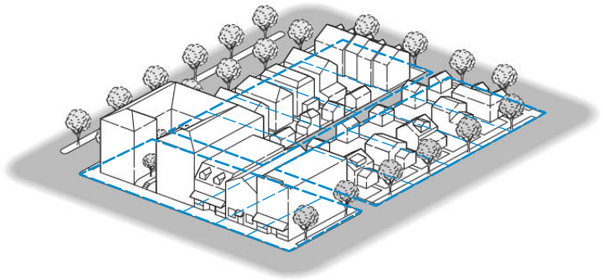
Streets

+ St. Paul 2040 Comprehensive Plan
+ Watershed requirements (RWMWD)
+ Hillcrest Master Plan
+ St. Paul Street Design Manual
+ District stormwater, utility, and traffic needs



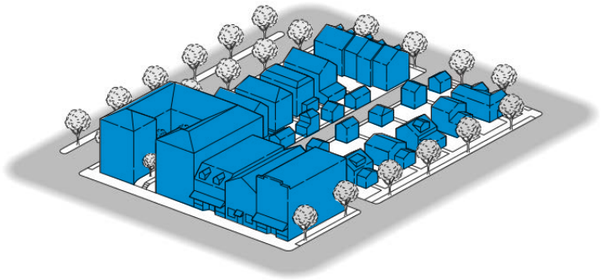
Blocks

+ Hillcrest Master Plan
+ City of St. Paul Zoning Code
+ City Council Ordinance 19-39: City Council Bonding Approval for Acquisition
+ LEED for Communities Platinum Pre-Certification



Lots

+ City of St. Paul Zoning Code
+ Watershed requirements (RWMWD)
+ Operations & Maintenance
+ Crime Prevention through Environmental Design (CPTED)
+ Market feasibility

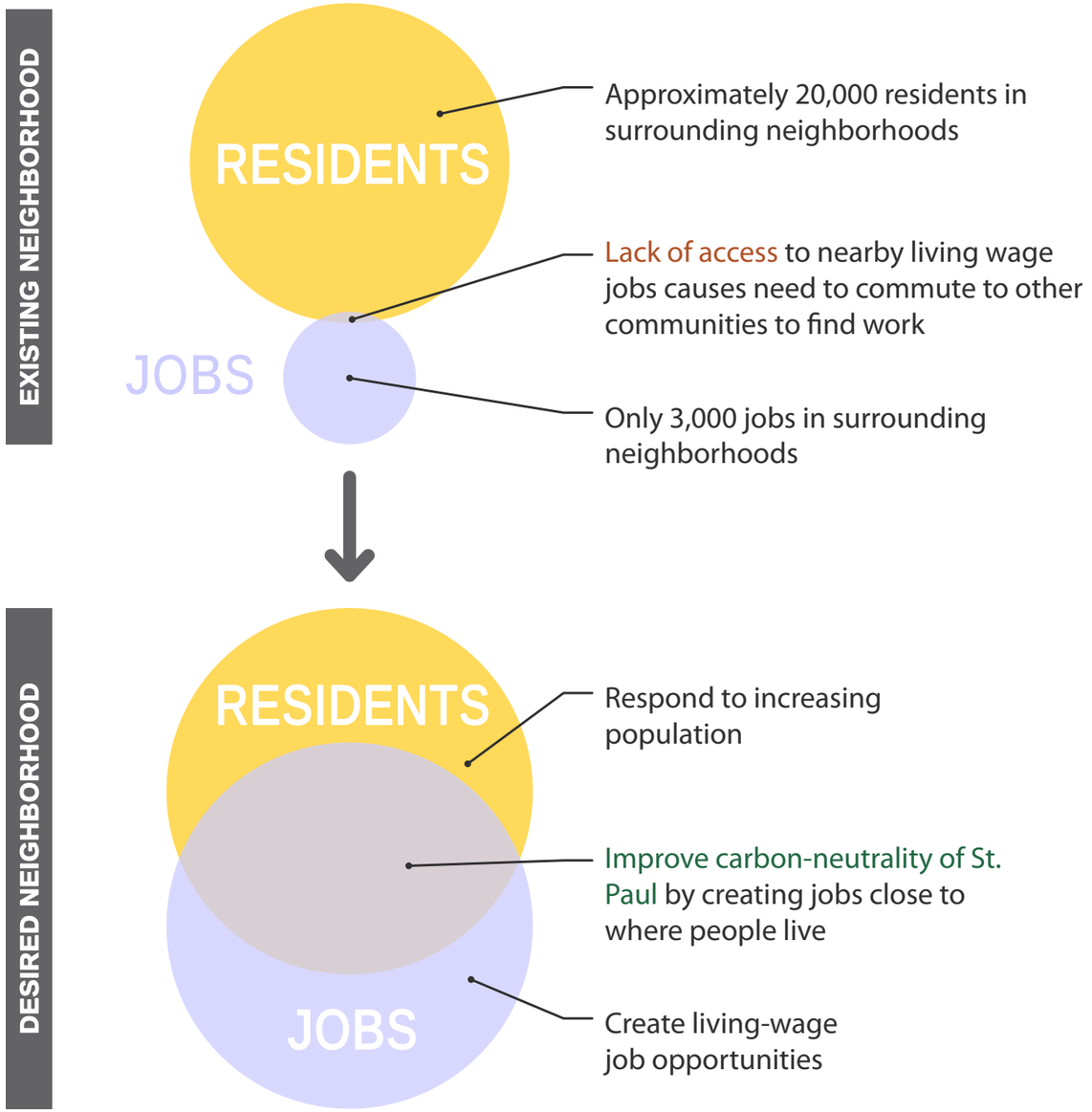


Buildings

+ City of St. Paul Building Code
+ LEED certification
+ Arts/Employment District concept
+ Sustainability requirements

PORT AUTHORITY GOALS

+ Create quality, living wage job opportunities
+ Expand the St. Paul tax base
+ Advance sustainable development

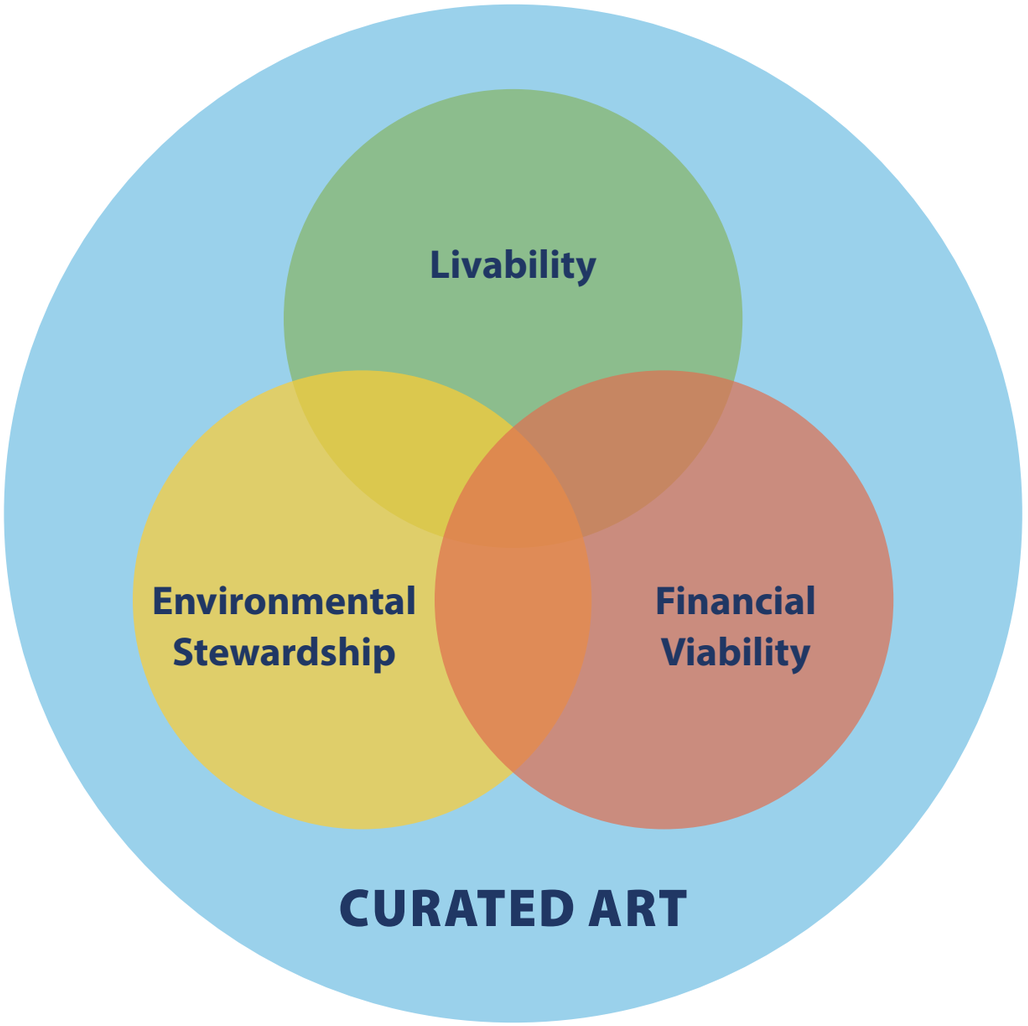


TYPICAL LIGHT INDUSTRIAL DEVELOPMENT

1. Based upon *market pressure* and the *project's goals* (i.e. create good jobs), it is clear that the least important factor for the success of the proposed light industrial development is the *architecture*.

2. Light industrial developers do not need impressive or beautiful buildings to run their businesses well, but the community benefits when these structures add nuance and character to the neighborhood.

3. All development projects should incorporate the goals of Livability, Environmental Stewardship, and Financial Viability.



WE WANT TO DO BETTER

The Port Authority has been seeking alternative compliance for building code requirements that accept mural-based public art as a form of architectural articulation.

"Murals and other art upon industrial buildings can satisfy the requirements of City Code Sec. 66.542(a)(2) in lieu of facade articulation if the Department of Safety and Inspections determines that the art has a similar visual impact from the street and can be adequately maintained."

From Hillcrest Master Plan, draft dated January 13, 2022, page 41

+ Any requirements that must be fulfilled by potential developers/ business owners must support the financial viability of their project.

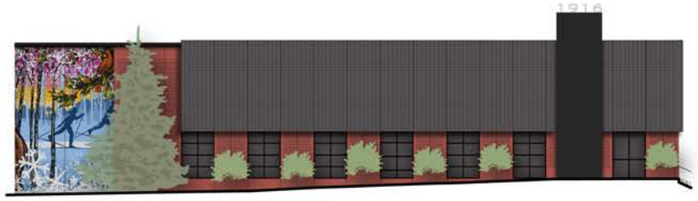
Additional requirements that increase the level of a site's environmental stewardship are also being explored.

LIVABILITY

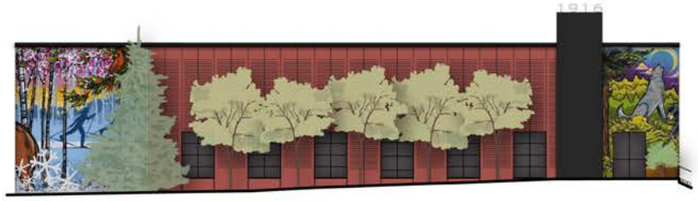
+ Layered, textured, human-scaled streetscape

FINANCIAL VIABILITY

+ Pre-selected, cost- and energy-efficient components
+ Create simple, attractive "background buildings" that are useful to job providers
+ Redirect investment from architecture to community-oriented site amenities



South-Facing Facade
15% Murals
850 Square Feet
$8,500-$34,000

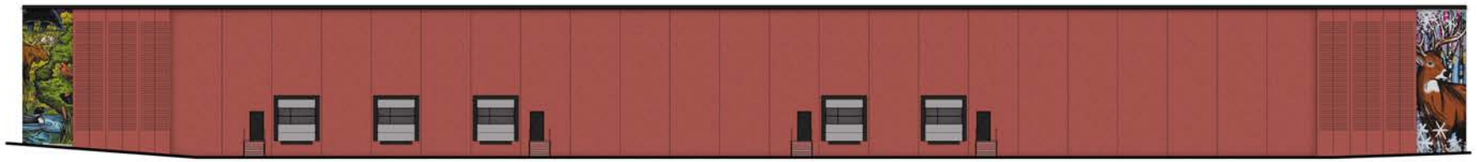


South-Facing Facade - No Solar
30% Murals *if Public-Facing*
1,700 Square Feet
$17,000-$68,000

Street Facade
20% Murals
2,400 Square Feet
$24,000-$96,000

Trail Facade
15% Murals
800 Square Feet
$8,000-$32,000

Loading Facade
6% Murals
850 Square Feet
$8,000-$32,000



ENVIRONMENTAL STEWARDSHIP

+ Stormwater infiltration and biofiltration
+ Energy – reserve roofs and surfaces for solar panels

A DIFFERENT APPROACH

to Light Industrial Building and Site Design

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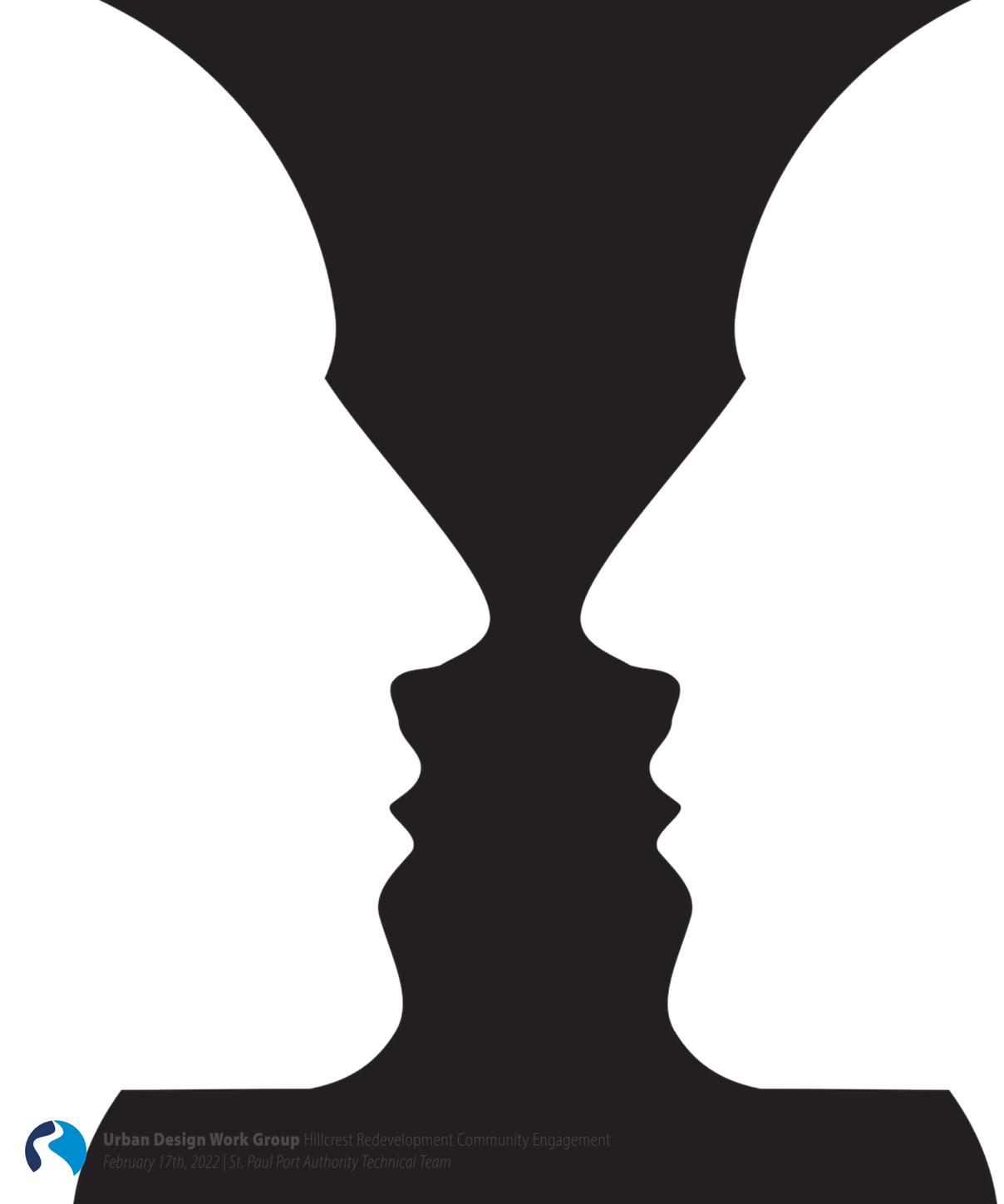
Saturnal Farm
Lexington, Ky



STREETS AND BLOCKS

They define each other's extents and character as they are each other’s negative space.

PLANNING DOCUMENTS & REQUIREMENTS

Saint Paul Comprehensive Plan

+ City-adopted document outlining community-driven policies that address city-wide physical development over the next 20 years
+ Establishes priorities for design of rights-of-way, with needs of pedestrians & bicyclists placed at the top

Maplewood Comprehensive Plan

+ City-adopted document outlining community-driven policies that address city-wide physical development over the next 20 years
+ Guides transportation planning while fitting strategically into regional visions laid out by the Metropolitan Council

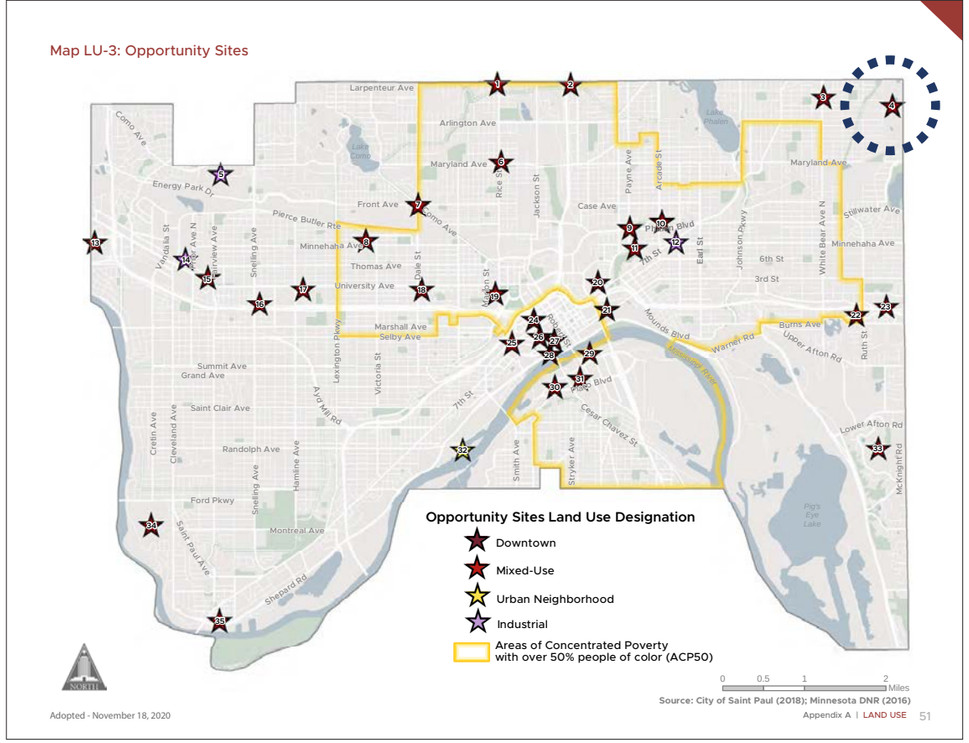
CITY OF ST. PAUL 2040 COMP PLAN

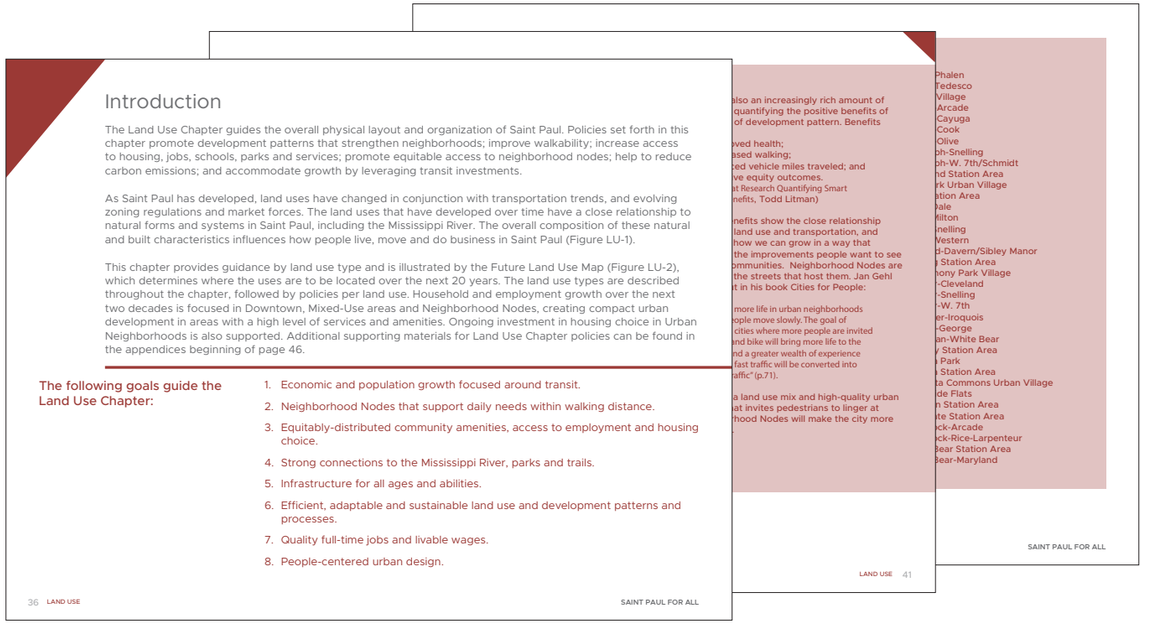
Overall city Land Use goals and Neighborhood Node locations are based on the urban design concept of the "20-minute city."

+ All residences within St. Paul shall be a 20-minute walk (or less) from a Neighborhood Node that offers a variety of amenities, such as neighborhood businesses, grocery stores, parks, and/or open space.

Hillcrest Golf Course is specifically mentioned on page 42 as a future Neighborhood Node location. Locations were identified via:

+ Adopted small area, neighborhood, and master plans
+ Community feedback
+ Market potential
+ Review of current zoning designations
+ Analysis of current and future land use

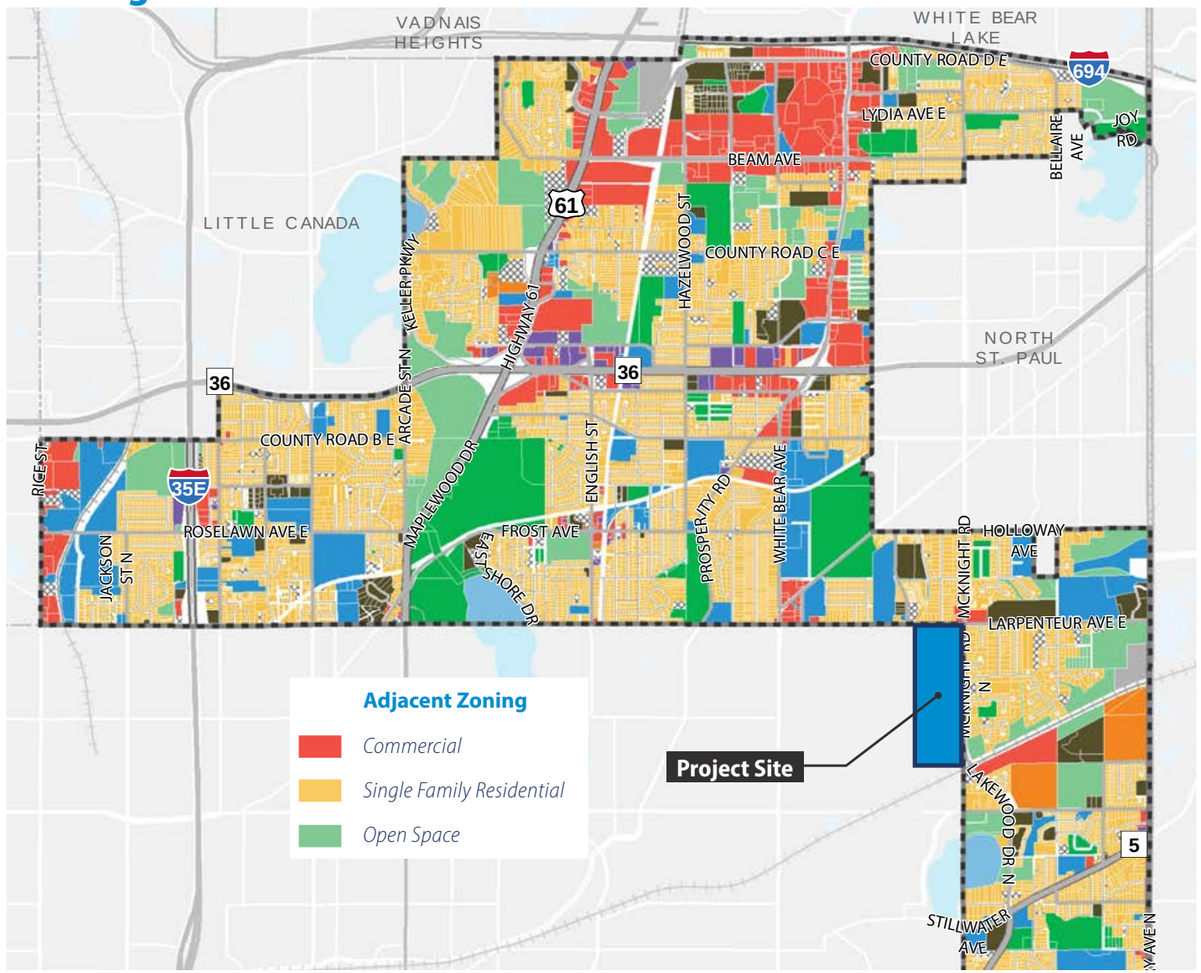




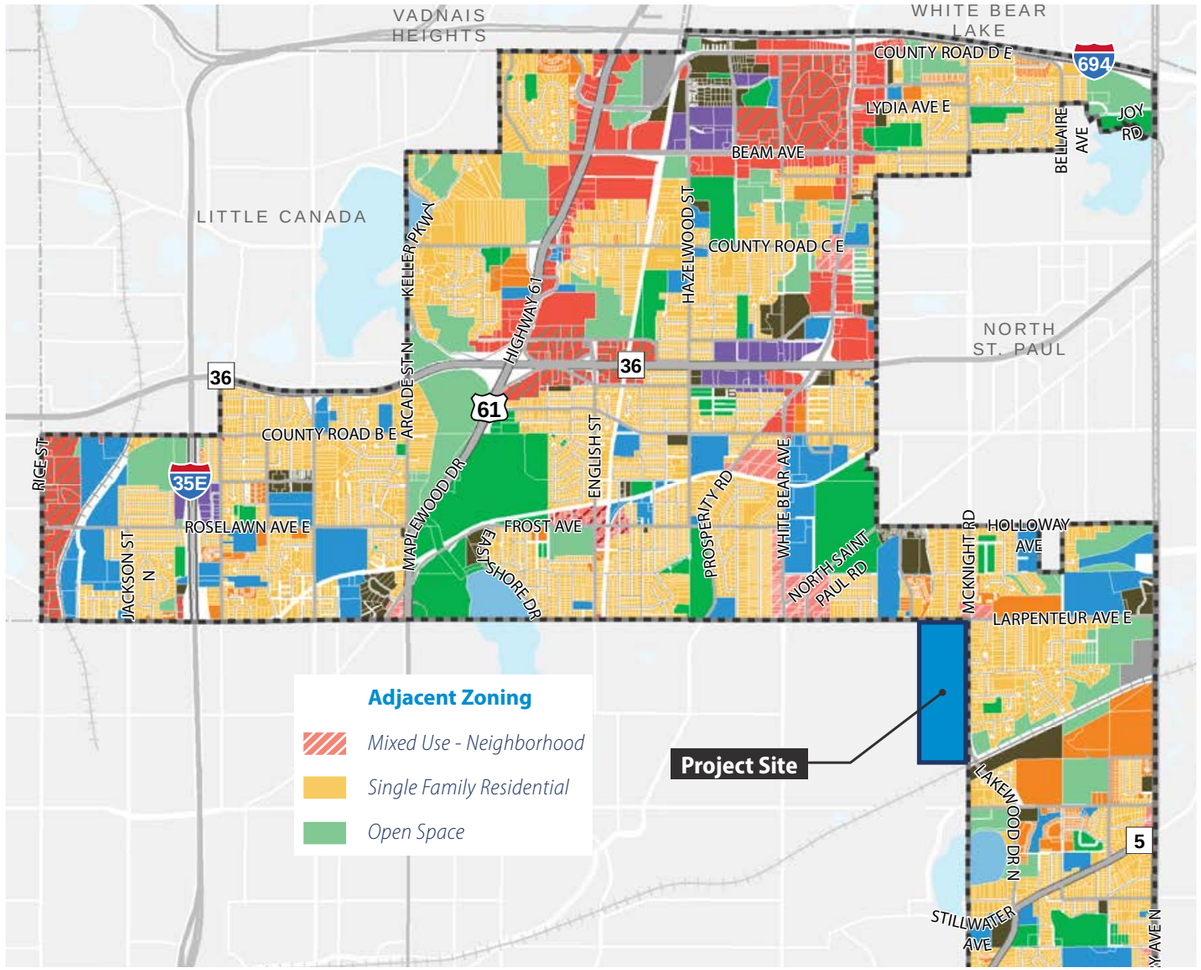
From City of St. Paul 2040 Comprehensive Plan

CITY OF MAPLEWOOD 2040 COMP PLAN

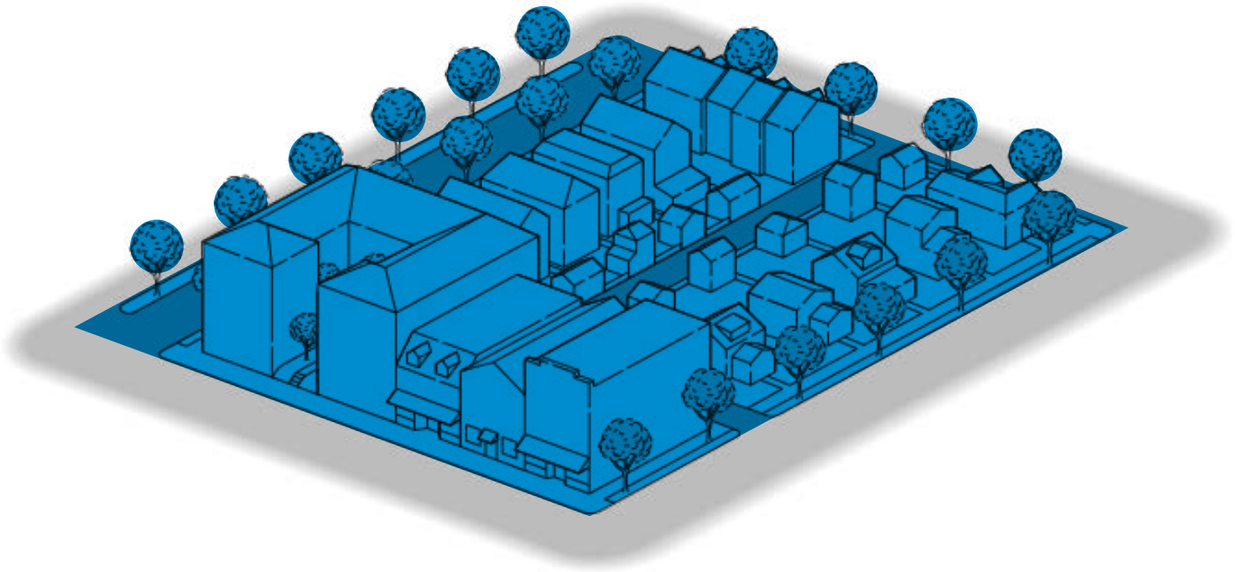
Existing Land Use



Future Land Use



From City of Minneapolis 2040 Comprehensive Plan



BLOCKS

1. Block configuration is strongly influenced by prioritizing alignment with the existing intersections around the site perimeter.

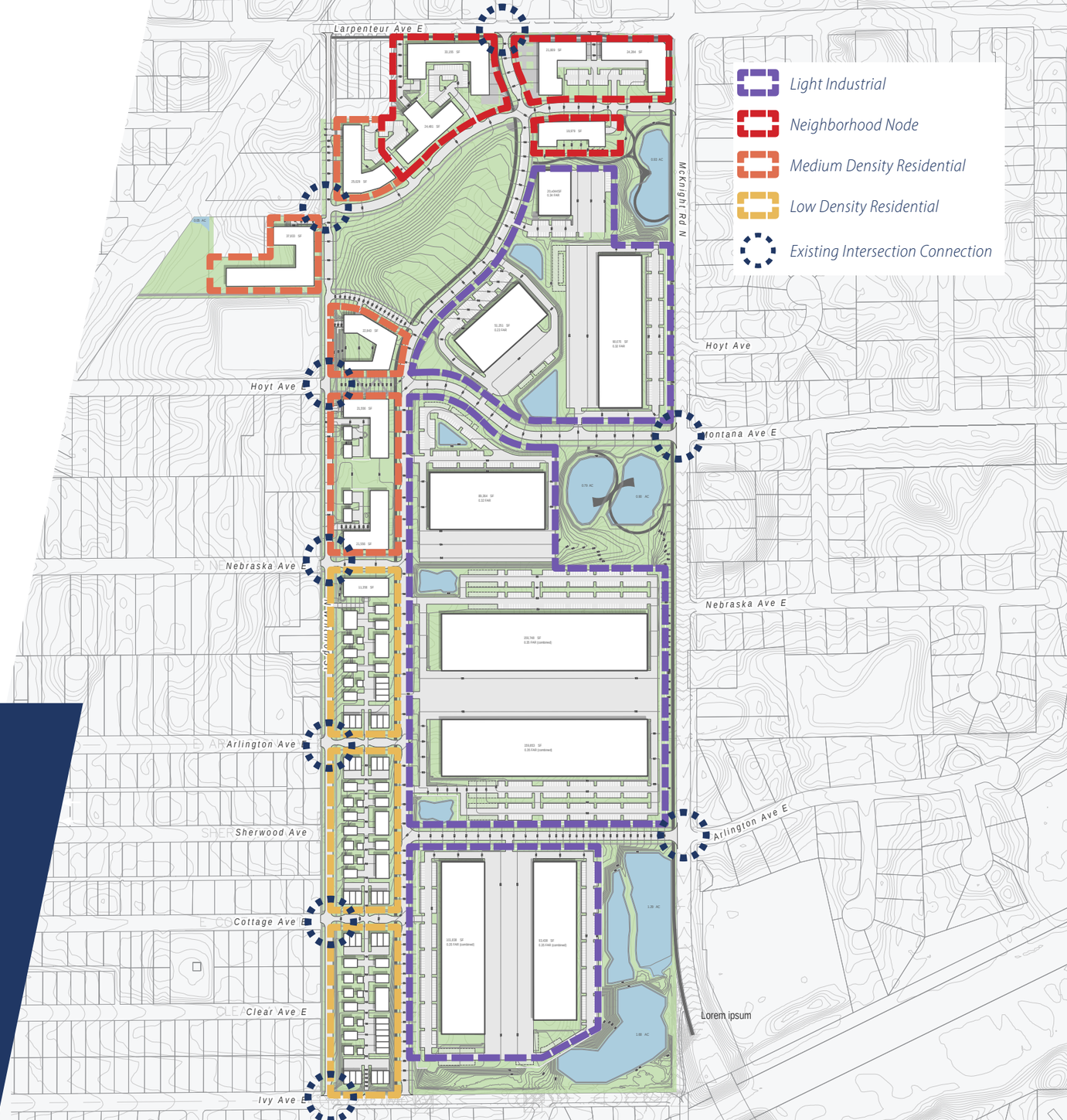
2. Blocks are further influenced by minimum and maximum dimensions for residential and mixed use development.

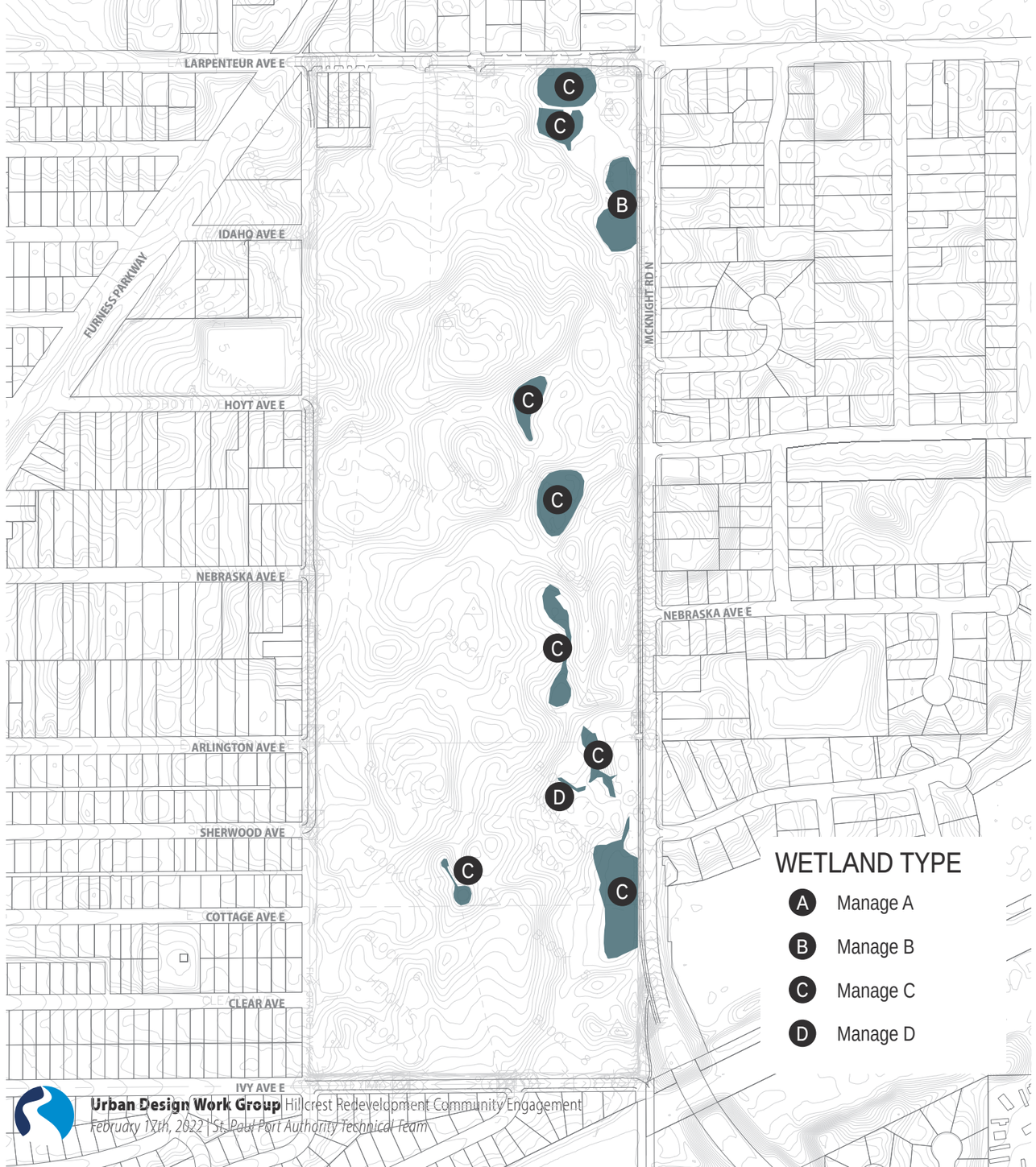
3. Light Industrial blocks are configured to maximize existing wetland preservation and long-term development flexibility.

MASTER PLAN

Block and Street Layout

+ City and County Traffic Engineers strongly endorsed a road network that aligns with existing intersections
  - Most truck traffic will be coming off of McKnight, entering the site at Arlington or Montana
  - Neighborhood Node main access at Larpenteur & Howard
+ Strong community desire to prevent cut-through traffic into the surrounding neighborhood
  - Every other street connects to the street grid west of the site
  - There are no continuous roads across the site east/west
  - Traffic calming/street diet measures will be used to discourage truck traffic from going west of Howard
+ Block depth at Neighborhood Node & western residential buffer zone reflect minimum developable dimensions





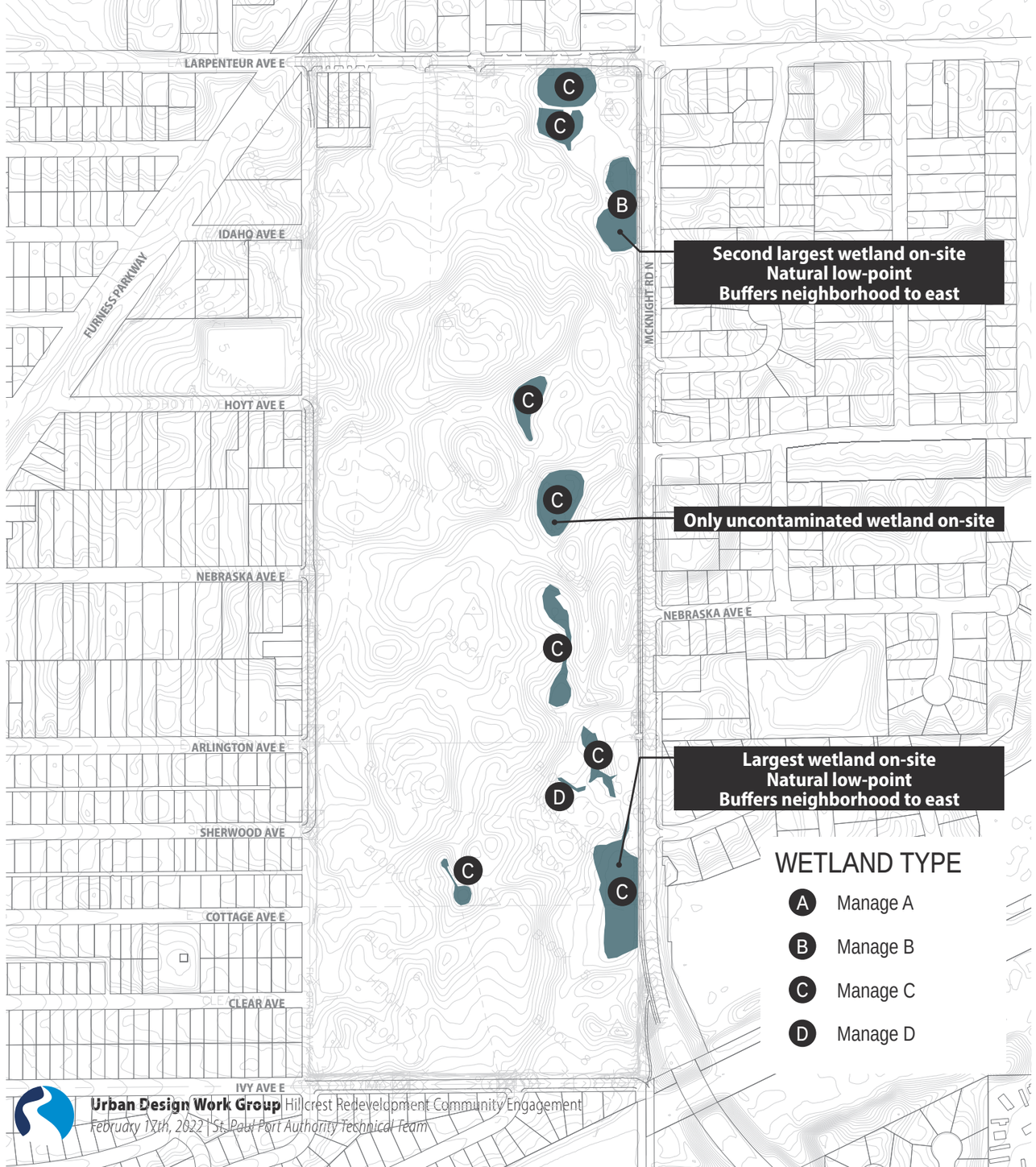
WETLAND MANAGEMENT

According to the Wetland Conservation Act (WCA), Ramsey-Washington Metro Watershed District (RWMWD), and the City of St. Paul:

Wetland Classification	Manage A	Manage B	Manage C	Not Yet Assessed
Average Buffer Width	75 feet	50 feet	25 feet	Wetlands will need to be assessed by project applicant to understand appropriate management level
Minimum Buffer Width	37.5 feet	25 feet	12.5 feet	

Wetland Terminology

+ Preservation: leave healthy wetlands alone
+ Restoration: clean-up contaminated wetlands in the current location
+ On-site Mitigation: off-set of unavoidable wetland impacts through creation of new wetlands, i.e. build healthy wetlands at a new location within the site.
  - Only replacement option available to fulfill watershed rules at this time, since watershed credits are not available
+ Off-site Mitigation: off-set unavoidable wetland impacts through creation of new wetlands, i.e. buy off-site credits or create new wetlands off-site



WETLAND MANAGEMENT

Necessary removal of contaminated soils within wetlands shall require dredging and restoration of wetlands within their existing footprint.

Watershed rules (RWMWD) are more restrictive than Wetland Conservation Act (WCA) requirements:

+ WCA requires 2:1 ratio for mitigated wetlands, but can be offset through state-purchased credits
+ Watershed requires 1:1 ratio for mitigated wetlands, but must be on-site

Change parcel along Larpenteur to high-density mixed-use

Eliminate ROW segment; block does not exceed 600' maximum; Covenants will address public pedestrian connection through high- and mid-density residential parcels

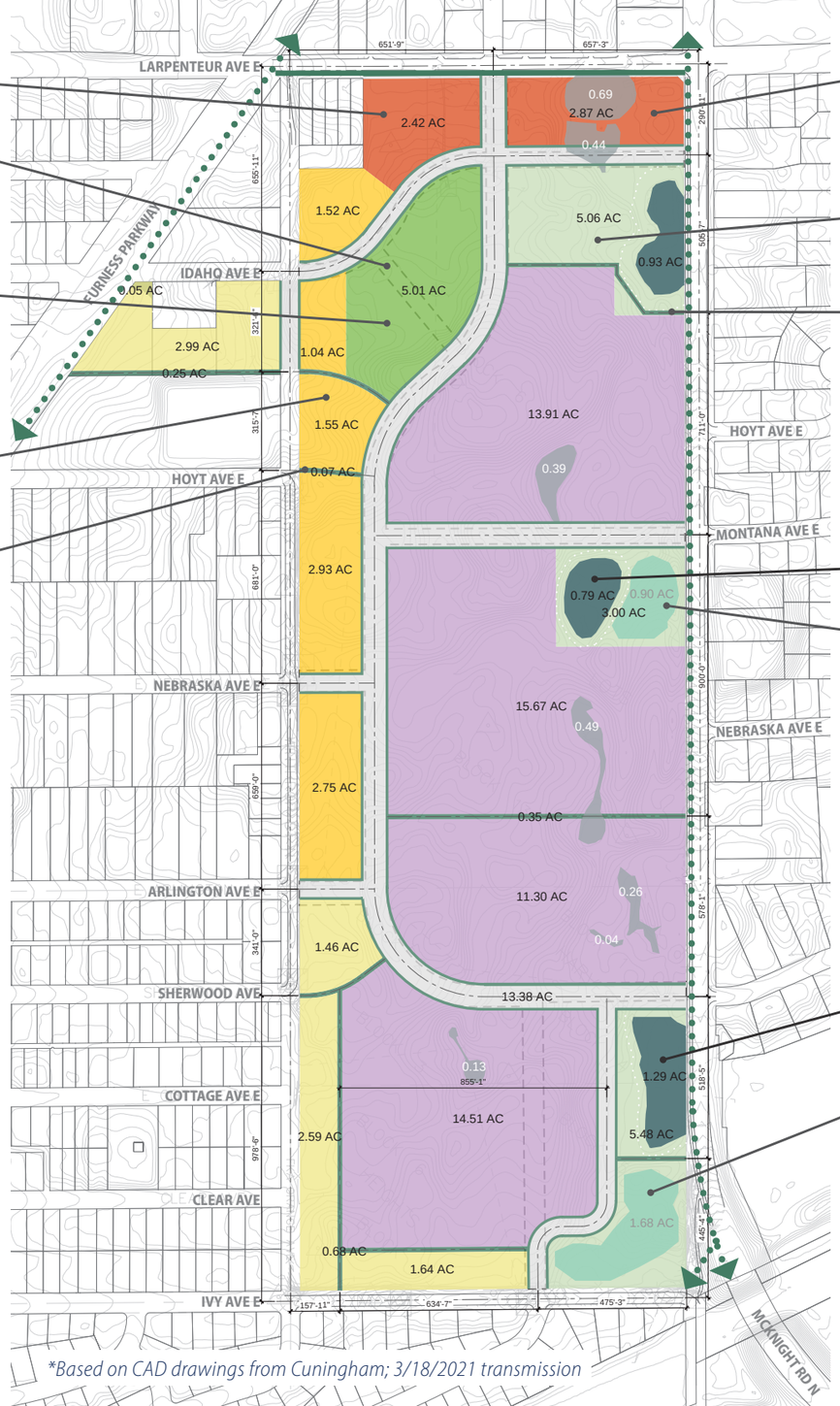
Shift Active Park to west side of spine road

Change parcels to mid-density residential

Shift trail northward to align with established street grid

LAND USE

	Original	Revised
	16.20 AC	8.68 AC
	11.4	9.79
	1.5	5.29
	54.75	55.22
	8.63	15.00
	4.77	5.01
	0.98 (17%)	3.06 (55%)
	4.62 (83%)	2.54 (45%)
	14.4	13.38
DU	903	925



*Based on CAD drawings from Cuningham; 3/18/2021 transmission

Change corner parcel to high-density mixed-use

Expand passive open space to reach 15 acre goal; good location for district stormwater management)

Shift trail and parcel boundary northward

Save Wetland D and 25' buffer (only non-contaminated wetland on site)

Locate mitigated wetlands (i.e. relocated wetlands) here within appropriate silty sand/clay sand soils

Save Wetland H and 25' buffer

Locate mitigated wetlands (i.e. relocated wetlands) here at the site's low ground and within hydric soils (clay sand)

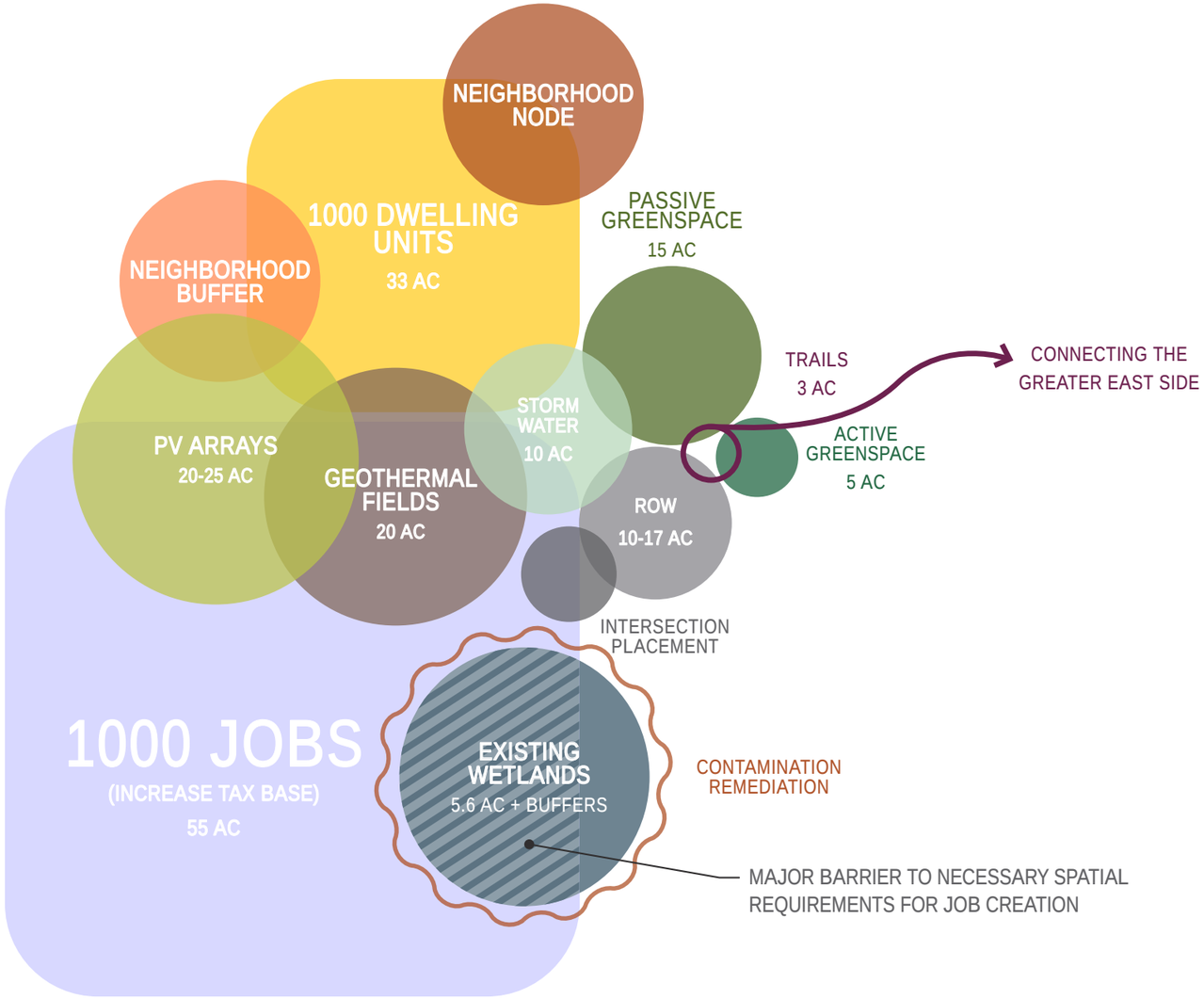
0 250 500 Ft

N

SPATIAL RELATIONSHIP BETWEEN USES

112 acre site

150+ acres of required programming

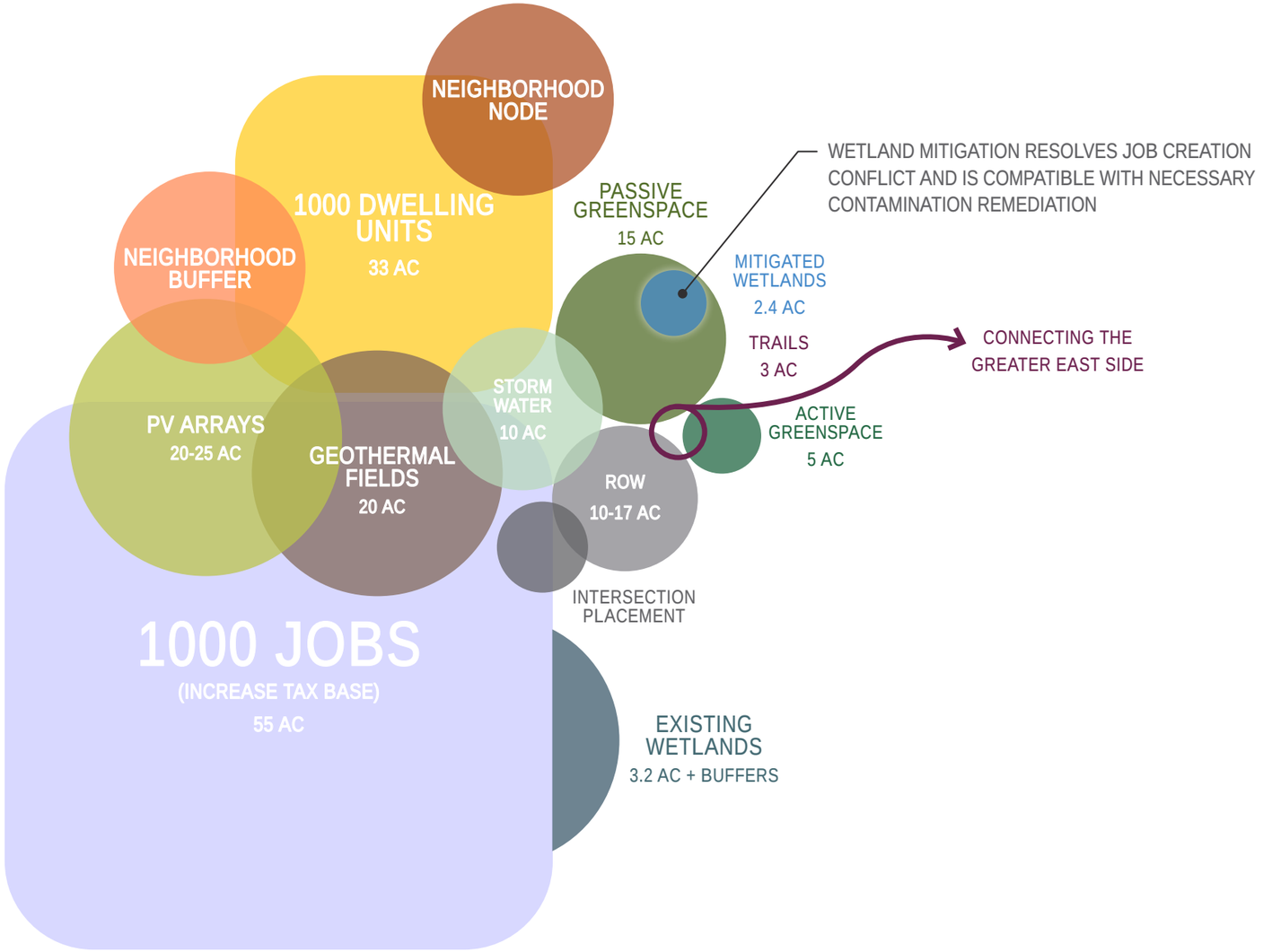


NOT TO SCALE

SPATIAL RELATIONSHIP BETWEEN USES

112 acre site

150+ acres of required programming



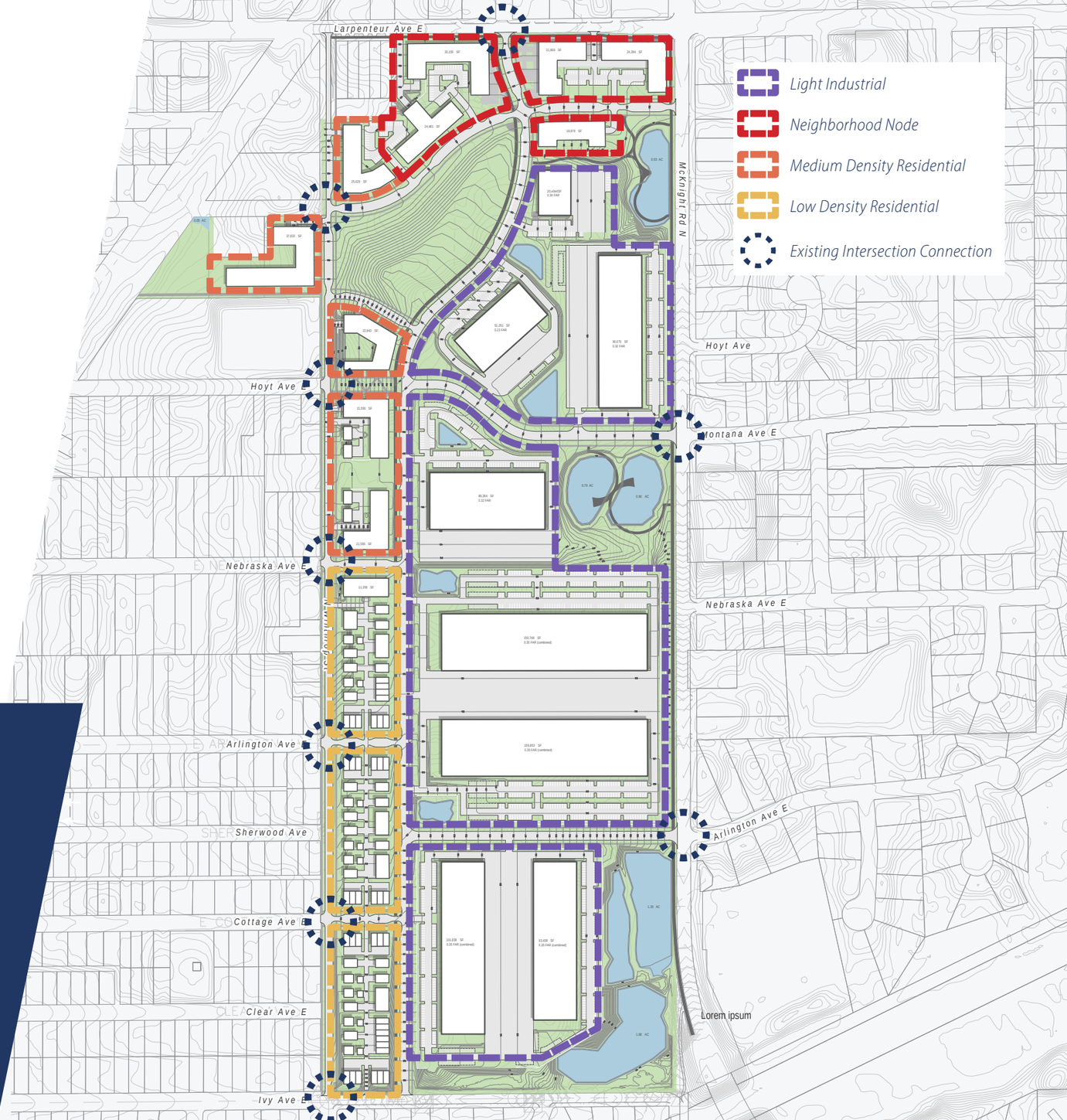
NOT TO SCALE

MASTER PLAN

Block Types and the Zoning Code

+ The Master Plan for Hillcrest designates four zoning types throughout the site:
  - T1 – Traditional Neighborhood
  - T2 – Traditional Neighborhood
  - T3 – Traditional Neighborhood
  - IT – Transitional Industrial
+ Each zoning type has specific requirements for principal uses and accessory uses, and provide direction on the density, scale, and appearance of development.
+ The size and scale of these blocks have a direct impact on the look and feel of the public realm.

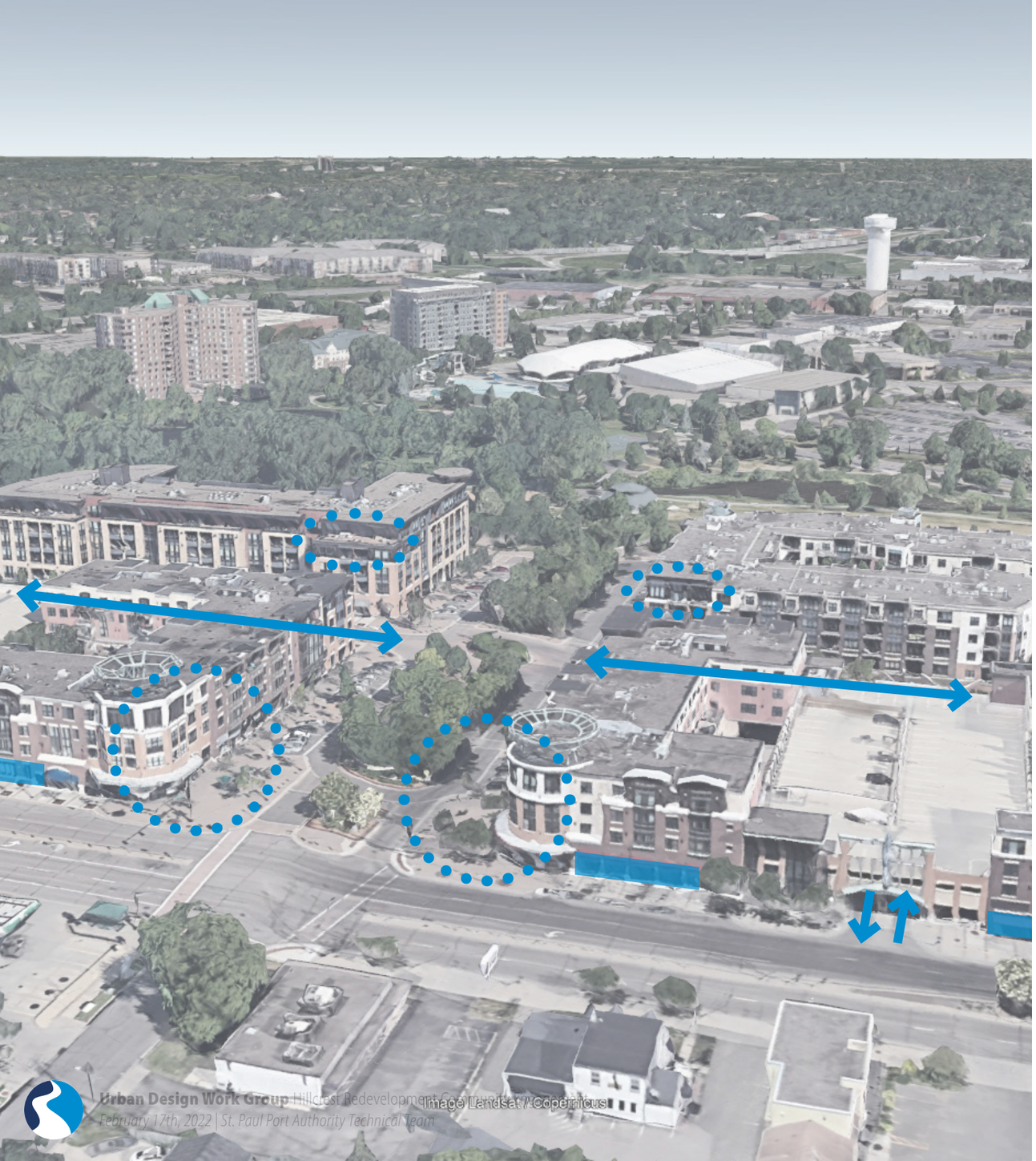
Refer to: library.municode.com/mn/st._paul/codes/code_of_



TRADITIONAL NEIGHBORHOOD DISTRICT DESIGN STANDARDS

(Sec. 66.343)

+ **Block length** (does not apply to T1) – in mixed use areas must not exceed 400' & in residential should generally match existing block patterns while not exceeding 660'
+ **Screening of equipment & service areas** (does not apply to T1) – fences, walls, or plantings at least 6' in height must screen storage, service, or loading equipment from public streets/walkways
+ Interconnected street & alley network (does not apply to T1) – extending & restoring the existing street network to the greatest degree possible (no cul-de-sac! Cul-de-sac bad!)
+ On-street parking (does not apply to T1) – generally prefer on-street parking lining both sides of the street to buffer pedestrians, calm traffic, and supplement off-street parking
+ **Off-street parking** – preferred to be within, below, or behind principal buildings, & if structured, must be buffered from public ROW with active uses and/or architectural screening
+ **Parking lot lighting** (does not apply to T1) – requires pedestrian-scaled lighting in off-street parking areas
+ Street trees – required within a minimum 5' wide strip (or approved structure) between the curb and sidewalk of all streets at regular intervals (~30' on center) to define edges, provide shade, & buffer pedestrians
+ Sidewalks – required on both sides of street except where abutting a park or other open space, & should be at least 6' wide in areas of high pedestrian activity, & at least 5' wide everywhere else
+ **Setbacks & front yard areas** – hard surfaced front yards should include amenities such as benches, tables, & planters



POTENTIAL AREAS OF INFLUENCE

How the lots work together to make a block

+ Corners & entry points
+ Private, semi-private, & public spaces
+ Open spaces within blocks – trail connections, pocket parks, POPS (privately-owned public space), community garden, temporary greenspace/agriculture

Scale & Density

+ Building heights, widths, parcel widths
+ Dwelling unit density per acre
+ Accessory Dwelling Units (ADU's)

Internal Circulation

+ Alleys & shared driveways
+ Driveway frequency

Rendering of potential POPS at Howard & Larpenteur

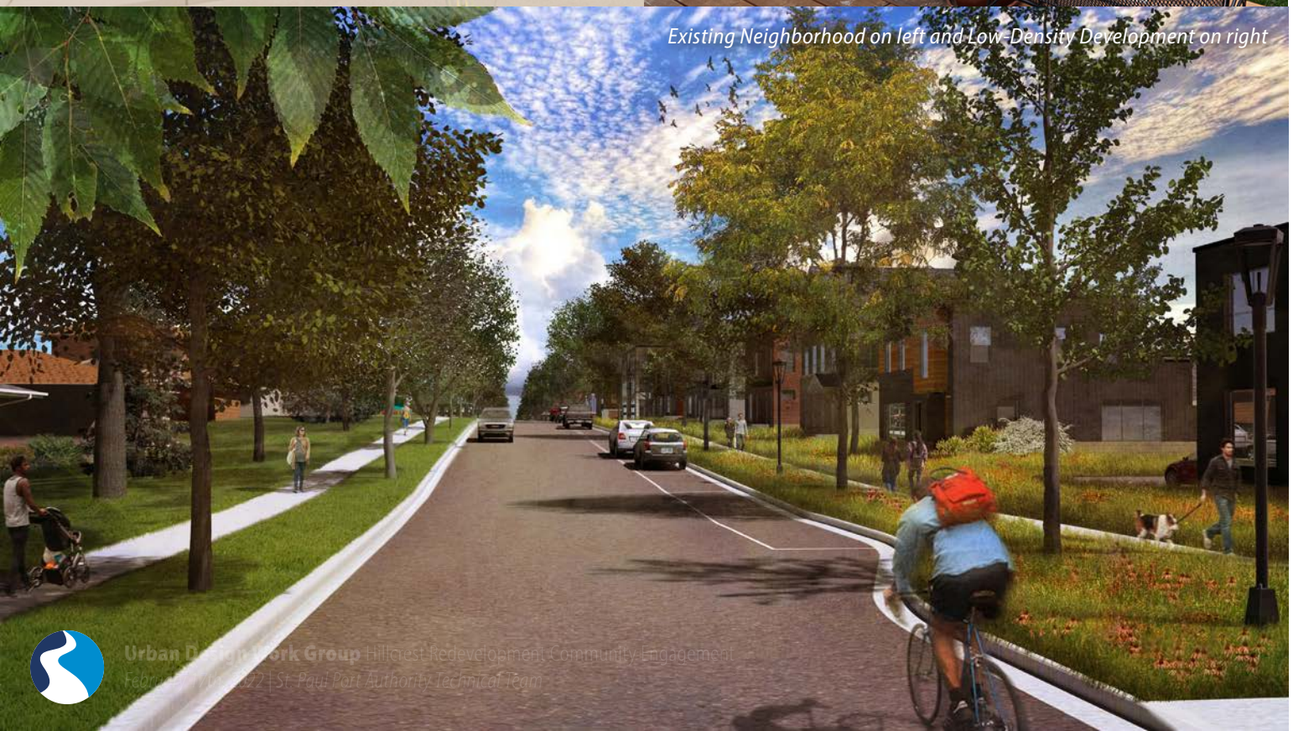
Rendering of Hoyt Greenway east of Winthrop

HOW LOTS WORK TOGETHER TO MAKE A BLOCK

+ Corners & Entry Points
+ Private, semi-private, and public spaces
+ Open spaces within blocks – trail connections, pocket parks, POPS (privately-owned public space), community garden, temporary greenspace/agriculture

5-Story Apartment building with step back

Accessory Dwelling Unit over a detached garage

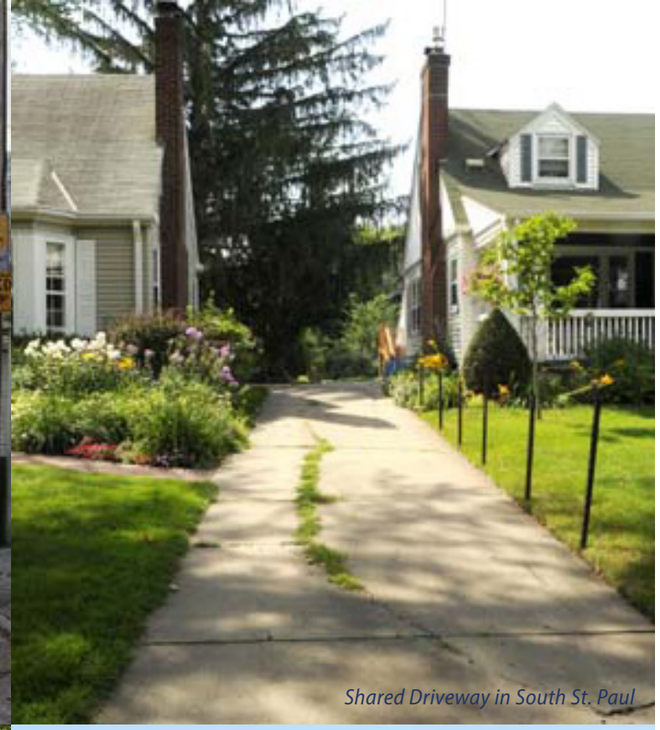


Existing Neighborhood on left and Low-Density Development on right

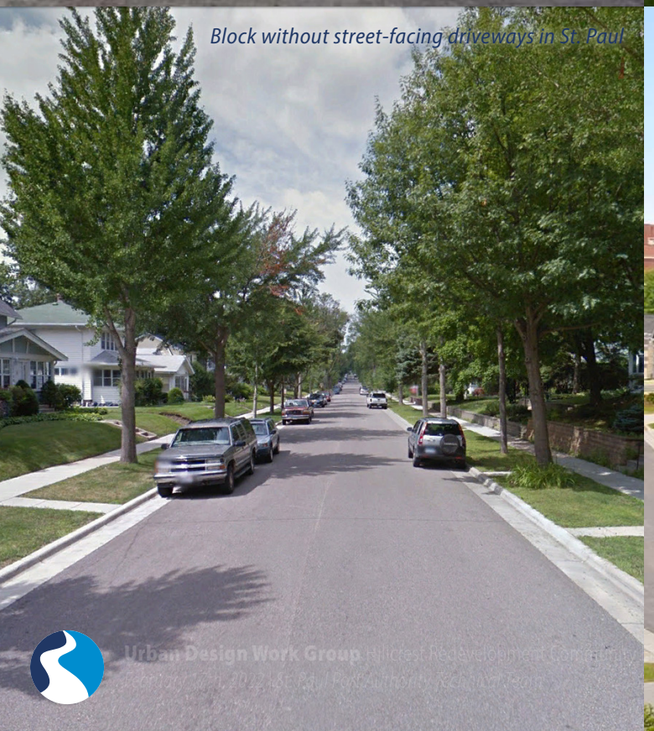
SCALE & DENSITY

+ Building heights, widths, parcel widths
+ Dwelling unit density per acre
  – Accessory Dwelling Units (ADU's)

Residential alley



Shared Driveway in South St. Paul



Block without street-facing driveways in St. Paul

Block with street-facing driveways

INTERNAL CIRCULATION

+ Alleys & shared driveways
+ Driveway frequency

HOMEWORK

Walkable, inviting spaces

+ Ensure all programmatic areas are accessible to all, keeping trip hazards and rolling barriers to a minimum
+ Natural materials and plantings are inviting, encouraging people to utilize an area
  – Generates "eyes on the street"

Architectural Interest

+ Variation in facade material
+ Projections, shadow reveals, parapets
+ Balconies and views

Lighting

+ Creates ambiance and enhances safety
+ https://www.pinterest.com/landscape_l/pathway-lighting/


submitted by Jennifer Brannen


submitted by Susan Bergmann


submitted by Susan Bergmann


submitted by Julie Guzman


submitted by Andrew Wise

HOMEWORK

Mixed-use, High Density

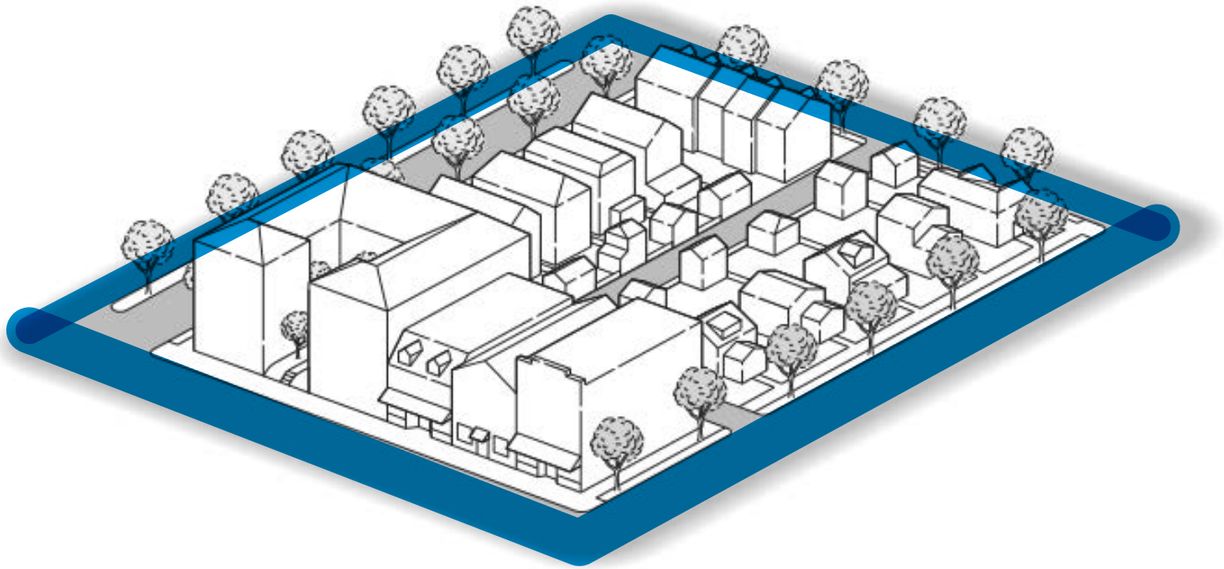
+ Street-level transparency
+ Connections through building mass

Neighborhood Node

+ Activation on all sides and edges
+ Permeable edges, so a person can approach from almost any direction
+ People love water!

Trails

+ Incorporate gathering spaces periodically along trail corridors



STREETS

1. There are several **planning documents & requirements** in place that outline the alignment & design of Saint Paul's Right of Way (ROW)

2. Different areas of Hillcrest require **different types of streets** to serve a variety of housing, commercial, light industrial, & recreational uses

3. Technical design considerations are essential to a functioning street network, but there are opportunities for **this group to inform** streetscape amenities, programming, & materiality.

HILLCREST MASTER PLAN STREET TYPES

Neighborhood Node Streets

+ Dedicates more space to pedestrian movement
+ Anticipates first-floor, mixed use programming
+ Best location for adjacent Privately-Owned Public Space (POPS)

Light Industrial Streets

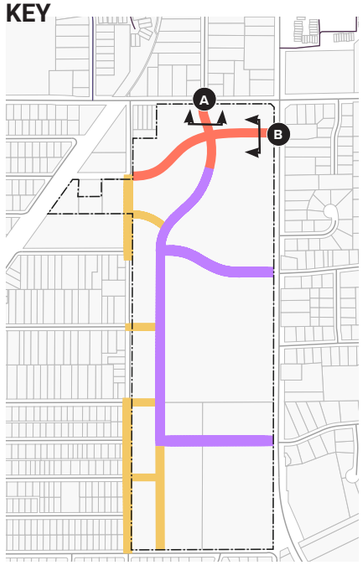
+ Accommodates pedestrian circulation, stormwater management, vegetative buffering, & public art
+ Must support truck movements to/from McKnight

Neighborhood Streets

+ Traffic calming is a priority
+ Focuses on pedestrian safety & comfort

STREET TYPES: NEIGHBORHOOD NODE

KEY



NORTH

NOTE:
For new developments and where opportunities are available to create additional setback, site designs should accommodate wider sidewalks with generous Boulevard/ Furnishing Zones. Source: Saint Paul Street Design Manual (p.23)

NODE - HOWARD

DIMENSIONAL CRITERIA

Minimum	Typical	
	82 ft	Right of Way Width
36 ft	38 ft	Pavement Width (includes gutter)
6 ft	8 ft	Sidewalk Width
6 ft	8 ft	Boulevard / Furnishing Width
2 ft		Frontage Zone
10 ft	12 ft	Two way bike trail

DESIGN FEATURES

- On-street parallel parking
- Intersection Bump-outs where feasible
- Street and Pedestrian-Scale Lighting
- Streetscape Furnishings (seating, planters,trash receptacles, bicycle racks)
- Tree-trench BMP's
- Street trees spaced 30'-40'

NODE - IDAHO

DIMENSIONAL CRITERIA

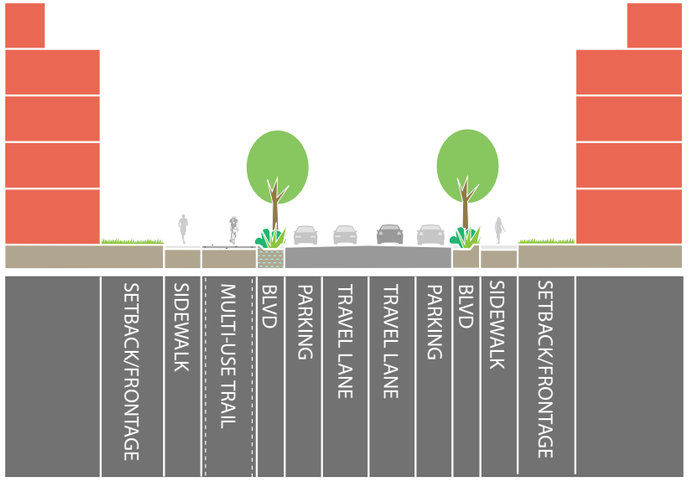
Minimum	Typical	
	70 ft	Right of Way Width
36 ft	38 ft	Pavement Width (includes gutter)
6 ft	8 ft	Sidewalk Width
6 ft	8 ft	Boulevard Width
2 ft		Frontage Zone

DIMENSIONAL CRITERIA

- On-street parallel parking
- Intersection Bump-outs where feasible
- Street and Pedestrian-Scale Lighting
- Streetscape Furnishings (seating, planters,trash receptacles, bicycle racks)
- Tree-trench BMP's
- Street trees spaced 30'-40'

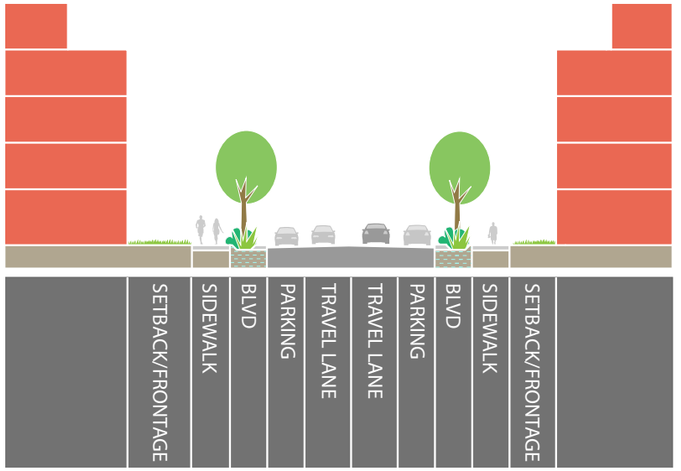
NODE - HOWARD STREET

A



NODE - IDAHO AVE

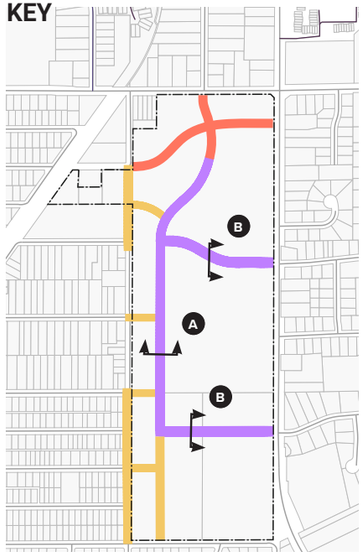
B



NOTE:
City Park on south side of Idaho west of Howard Street

STREET TYPES: LIGHT INDUSTRIAL

KEY



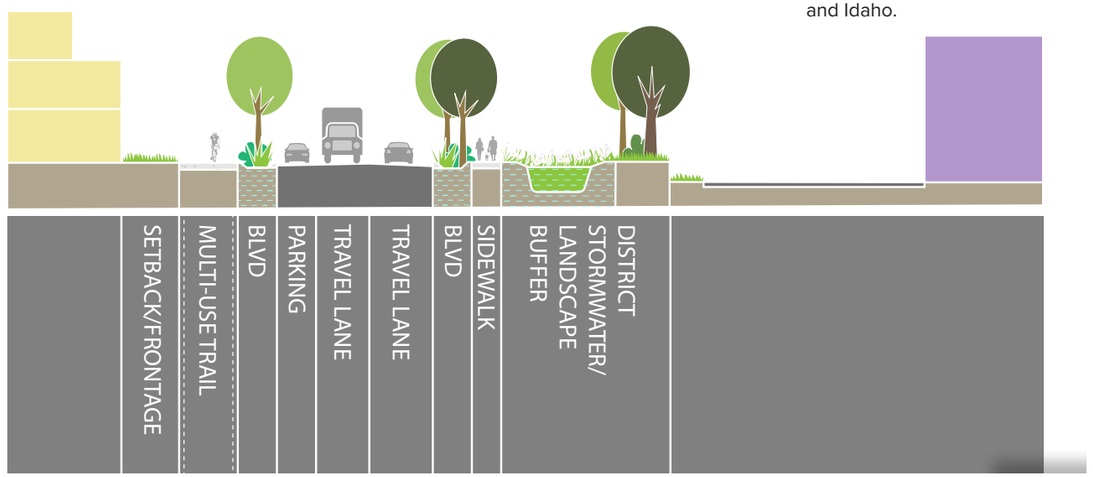
NORTH

LIGHT INDUSTRIAL - HOWARD		
DIMENSIONAL CRITERIA		
Minimum	Typical	
	80 ft	Right of Way Width (does not include District Stormwater/Landscape Buffer)
36 ft	40 ft	Pavement Width (includes gutter)
6 ft	6 ft	Sidewalk Width
6 ft	8 ft	Boulevard / Furnishing Width (see note)
2 ft		Frontage Zone (west side only)
10 ft	12 ft	Multi-use Trail
25 ft	25 ft	District Stormwater / Landscape Buffer
DESIGN FEATURES		
» District Stormwater / Landscape buffer feature along east side of street		
» On-street parallel parking on west side of street		
» Intersection bumpouts where feasible		
» Street and Pedestrian-Scale Lighting		
» Tree-trench BMP's		

LIGHT INDUSTRIAL - HOWARD STREET

NOTE:
City Park on west side of Howard, between Hoyt and Idaho.

A



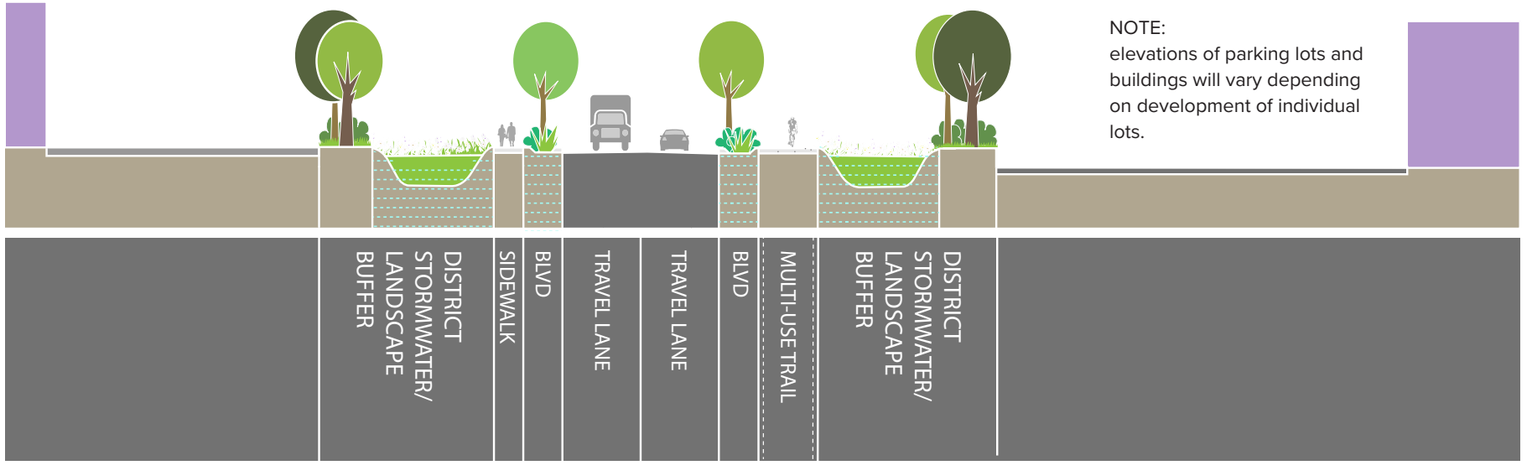
LIGHT INDUSTRIAL - ARLINGTON AND MONTANA		
DIMENSIONAL CRITERIA		
Minimum	Typical	
	80 ft	Right of Way Width (does not include District Stormwater/Landscape Buffer)
32 ft	40 ft	Pavement Width (includes gutter)
6 ft	6 ft	Sidewalk Width
6 ft	8 ft	Boulevard / Furnishing Width (see note)
10 ft	12 ft	Multi-use Trail
25 ft	25 ft	District Stormwater / Landscape Buffer
DESIGN FEATURES		
» District Stormwater / Landscape buffer		
» Intersection bumpouts where feasible		
» Street and Pedestrian-Scale Lighting		
» Tree-trench BMP's		

LIGHT INDUSTRIAL - ARLINGTON AND MONTANA AVE

NOTE:
boulevard width may need to be expanded to truck turning needs at driveways.

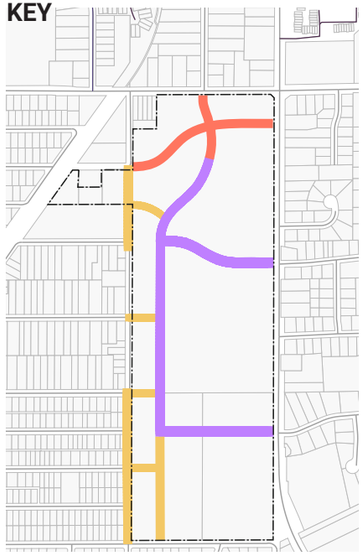
NOTE:
elevations of parking lots and buildings will vary depending on development of individual lots.

B



STREET TYPES: NEIGHBORHOOD STREETS

KEY



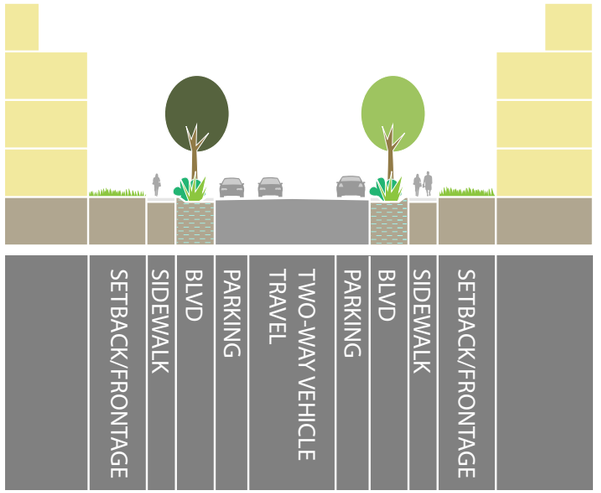
NORTH

NEIGHBORHOOD - TYPICAL		
DIMENSIONAL CRITERIA		
Minimum	Typical	
	60 ft	Right of Way Width
29 ft	32 ft	Pavement Width (includes gutter)
6 ft	6 ft	Sidewalk Width
5 ft	8 ft	Boulevard / Furnishing Width
0 ft	2 ft	Frontage Zone
DESIGN FEATURES		
» On-street parallel parking in front of new residential		
» Intersection bumpouts where feasible		
» Street and Pedestrian-Scale Lighting		
» Streetscape Furnishings (seating, planters,trash receptacles, bicycle racks) adjacent to residential		
» Tree-trench BMP's		

NEIGHBORHOOD - IOWA AND HOWARD		
DIMENSIONAL CRITERIA		
Minimum	Typical	
	66v ft	Right of Way Width
32 ft	32 ft	Pavement Width (includes gutter)
6 ft	6 ft	Sidewalk Width
6 ft	8 ft	Boulevard / Furnishing Width
10 ft	12 ft	Multi-use Trail
0 ft	2 ft	Frontage Zone
DESIGN FEATURES		
» On-street parallel parking in front of new residential		
» Intersection bumpouts where feasible		
» Street and Pedestrian-Scale Lighting		
» Streetscape Furnishings (seating, planters,trash receptacles, bicycle racks) adjacent to residential		
» Tree-trench BMP's		

NEIGHBORHOOD - TYPICAL

A



NOTE
Refer to land use plan for housing density.

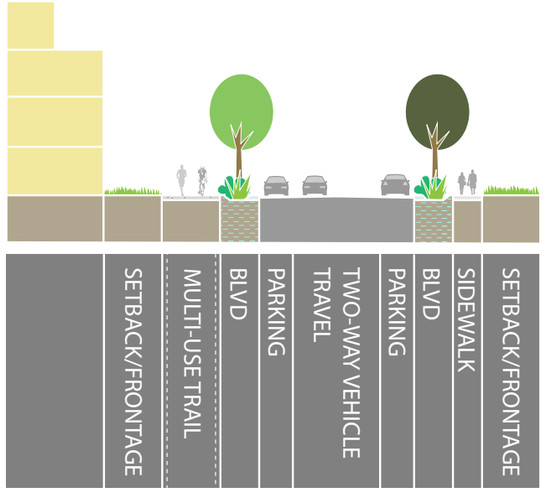
NOTE:
Off site green space on west side of Winthrop, between Hoyt and south west corner of City Park.

NOTE:
City Park on east side of Winthrop south of Idaho.

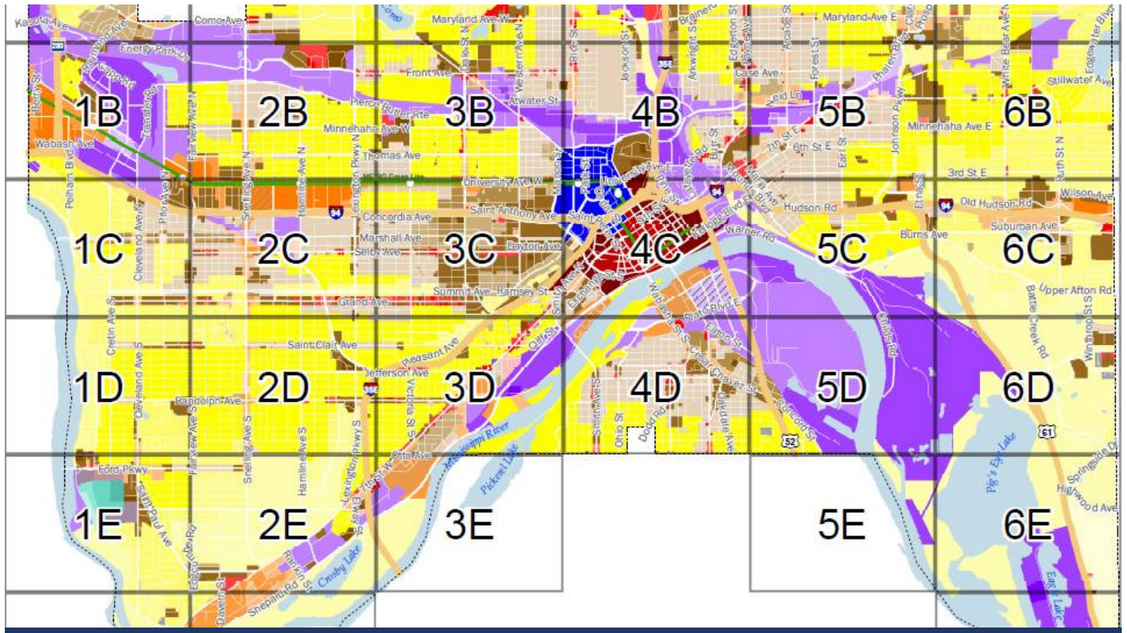
NOTE:
Winthrop Street south of Sherwood may swing east within the right-of-way as feasible, if its centerline aligns with Orange's centerline at Ivy. This is the only portion of Neighborhood Streets expected to potentially be at the minimum dimensions for pavement, sidewalk, and boulevard widths.

NEIGHBORHOOD - IOWA AND HOWARD

B



PLANNING DOCUMENTS & REQUIREMENTS



Saint Paul Zoning Code

+ Traditional Neighborhood District Regulations (Code of Ordinances Part II, Title VIII, Chapter 66, Article III)

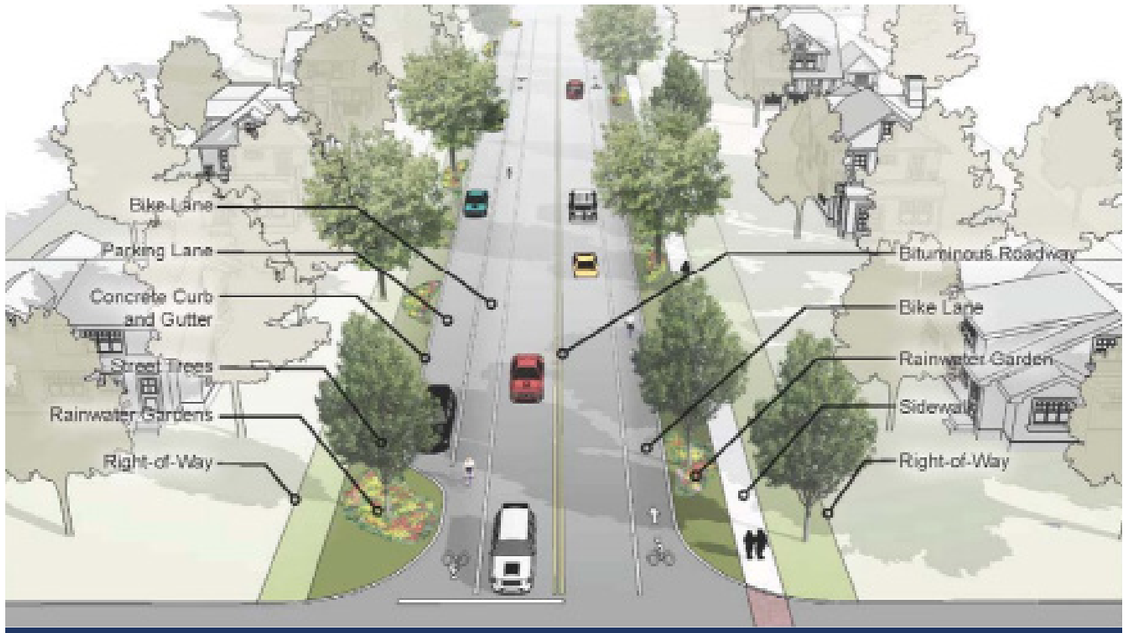
Complete Streets

+ Defined by the U.S. Department of Transportation as streets designed & operated to enabled safe use & support mobility for all users (regardless of age, ability, or mode of transportation)
+ Policies are set at state, regional, & local levels to guide balanced systems

PLANNING DOCUMENTS & REQUIREMENTS

Saint Paul Street Design Manual

+ Result of a national grant, this manual illustrates street design best practices & implements adopted complete streets policies
+ Intended to streamline the City's internal street design processes



Maplewood Living Streets Policy

+ Combines "Complete Streets" & "Green Streets" methodologies to provide multiple modes of transport while reducing environmental impacts
+ City's principal guide for evaluating & implementing all multi-modal transportation improvement projects, including reconstruction & new construction

PLANNING DOCUMENTS & REQUIREMENTS

Sustainability Requirements

+ District stormwater, district geothermal, & district energy are all being discussed & designed by other consultant groups & have elements that intersect the right of way (i.e. space needs to be saved for them)

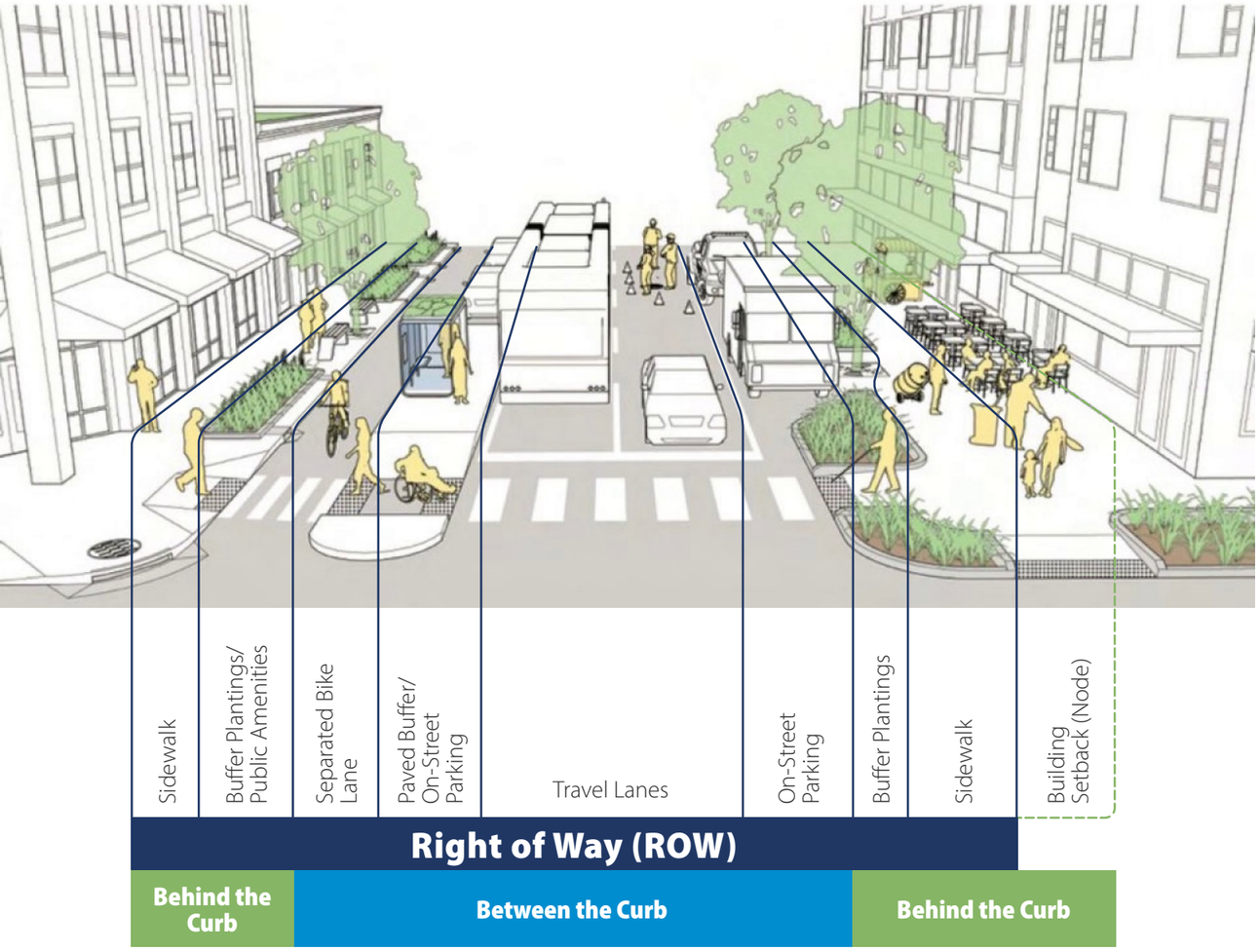
Other Standards & Guidelines

+ Metropolitan Council Thrive 2040
+ Metropolitan Council Transportation Plan
+ City of Saint Paul 2040 Comprehensive Plan
+ City of Maplewood 2040 Comprehensive Plan
+ Saint Paul Standard Plates
+ Street and Park Tree Master Plan
+ Parks and Recreation System Plan
+ Saint Paul Bicycle Plan
+ Saint Paul Street Lighting Policy
+ Blooming Saint Paul
+ Brick Street Policy
+ Public Art Ordinance Guidelines

TRADITIONAL NEIGHBORHOOD DISTRICT DESIGN STANDARDS

(Sec. 66.343)

+ **Block length** (does not apply to T1) – in mixed use areas must not exceed 400' & in residential should generally match existing block patterns while not exceeding 660'
+ **Screening of equipment & service areas** (does not apply to T1) – fences, walls, or plantings at least 6' in height must screen storage, service, or loading equipment from public streets/walkways
+ **Interconnected street & alley network** (does not apply to T1) – extending & restoring the existing street network to the greatest degree possible (no cul-de-sac! Cul-de-sac bad!)
+ **On-street parking** (does not apply to T1) – generally prefer on-street parking lining both sides of the street to buffer pedestrians, calm traffic, and supplement off-street parking
+ **Off-street parking** – preferred to be within, below, or behind principal buildings, & if structured, must be buffered from public ROW with active uses and/or architectural screening
+ **Parking lot lighting** (does not apply to T1) – requires pedestrian-scaled lighting in off-street parking areas
+ **Street trees** – required within a minimum 5' wide strip (or approved structure) between the curb and sidewalk of all streets at regular intervals (~30' on center) to define edges, provide shade, & buffer pedestrians
+ **Sidewalks** – required on both sides of street except where abutting a park or other open space, & should be at least 6' wide in areas of high pedestrian activity, & at least 5' wide everywhere else
+ **Setbacks & front yard areas** – hard surfaced front yards should include amenities such as benches, tables, & planters



POTENTIAL AREAS OF INFLUENCE

Public Amenities

+ Frequency & character of site elements (benches, planters, etc.)
+ Lighting character

Planting Strategies

+ Turf grass vs. native plantings

Nodes/Intersections

+ Adjacent programming, facade variation (including bays, awnings), wayfinding, etc.

Paving Strategies

+ Use delineation, materiality (asphalt vs. brick), traffic calming

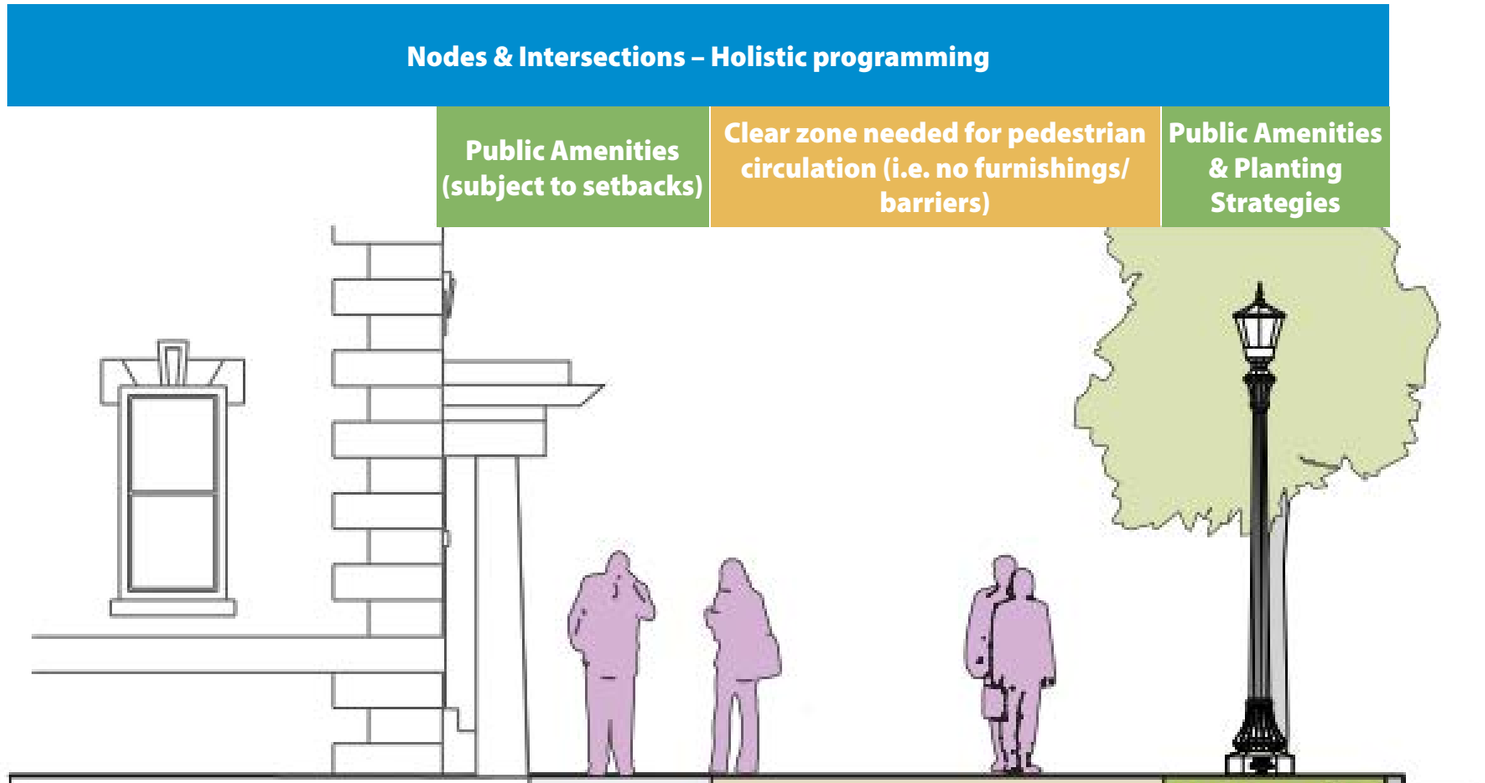
Traffic Calming Measures

+ Bump-outs, speed bumps, tabled intersections, shared spaces, buffer requirements

BEHIND THE CURB

Potential Areas of Influence

+ **Frontage Zone –** area that buffers building entrances/provides space for sidewalk cafes, window shopping, or landscaping
+ **Pedestrian Zone –** area specifically reserved for pedestrian circulation that must be clear of barriers like furnishings or plantings
+ **Boulevard & Furnishings Zone –** area for vegetation, utilities, bike parking, street furniture that buffers pedestrians from roadway
+ **Curb Zone –** separation zone between travel lanes/other zones that can be raised or at grade



STREET TYPE	Frontage Zone		Pedestrian Zone		Boulevard & Furnishings Zone		Curb & Gutter (varies)
	Pref.	Min. des.	Preferred	Minimum Desirable	Pref.	Min. des.	
Mixed-Use Corridor Streets	2'	0'	8'	6'	6'	5'	
Residential Corridor Streets	2'	0'	5'	5'	6'	5'	
Neighborhood Streets	2'	0'	6'	6'	6'	5'	
Industrial Streets	2'	0'	5'	5'	6'	5'	

Zone Definition & Widths from Saint Paul Street Design Manual

Street with trees, benches, trash & recycling receptacles, & lighting

Edible Streetscape Project (E 7th Street)

Street with planters, benches, & bike racks

Artistic stop sign post

PUBLIC AMENITIES

+ Frequency of site elements, like benches, trash/recycling receptacles, bike racks, etc., can greatly influence how it feels to use a street as a pedestrian
+ Character of site elements (i.e. modern, traditional, etc.)
+ Incorporation of placemaking or public art elements

Street with native plantings & canopy trees

Street with turf grass & ornamental trees

PLANTING STRATEGIES

+ Trees must have an 8' minimum vertical clearance from the sidewalk to the lowest branch
+ Boulevard plantings must not exceed 3' in height (12" is the maximum planting height at, & within 30' of, intersections)
+ Salt & drought tolerance are essential in plant selection
+ "Door zone" of parked cars (~2') should be clear of plantings
+ Maintenance & operations considerations
+ Green stormwater infrastructure – range of stormwater management features like bioswales, rain gardens, etc. that capture, infiltrate, & clean stormwater runoff – part of district stormwater system

Privately-Owned Public Space
(Andrew's Submission)

Street with sidewalk cafes & vegetative buffer

NODES & INTERSECTIONS

+ Adjacent programming expressed in the streetscape (i.e. sidewalk cafes & parklets)
+ Facade variation (including bays & awnings)
+ Wayfinding elements like kiosks & unique elements
+ Activity centers – where modes & uses converge
+ Crosswalks
+ Privately-Owned Public Space (POPS)

Street where the temporary event space area is paved with bricks, making it stand out from the rest of the street network (Rachel's Homework)

Street with permeable paver parking strip

PAVING STRATEGIES

+ Paving selections can differentiate between modes of transportation or street types
+ Different materials have different cost, maintenance, & operational considerations (i.e. asphalt vs. pavers)
+ Permeable pavement can be incorporated in low traffic areas (i.e. parking lots/spaces) to reduce impervious surface area

Bumpout with stormwater infrastructure

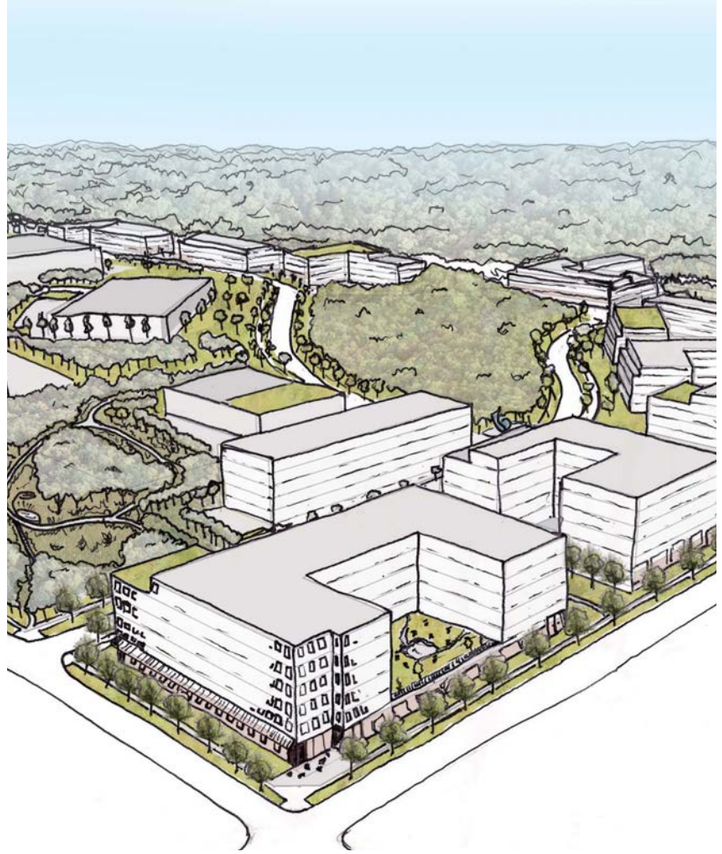
Intersection prioritizing pedestrian traffic on a street with paving variation/bumpouts/plantings to separate traffic modes

TRAFFIC CALMING MEASURES

+ On street parking
+ Speed bumps/humps
+ Bump outs at intersections and/or crosswalks
+ Tabled intersections
+ Shared spaces (i.e. woonerfs)
+ Paving strategies to identify/distinguish transportation modes
+ Buffer requirements like bollards, medians, planting strips, elevation differences

WORK GROUP AREAS OF INTEREST

+ Pedestrian experience
+ Transit
+ Building massing and materiality
+ Cohesively blending with the existing neighborhoods
+ Common areas and green spaces
+ Housing design
+ Local work opportunities
+ Green space
+ Street design
+ Neighborhood node (no fast food please)
+ High density housing and crime prevention design



THANK YOU!

Hillcrest Redevelopment Community Engagement
St. Paul Port Authority Technical Team